



# MONARCH GREEN

Land at Foxlydiate Lane, Webheath





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*Chapter 1:*

# MONARCH GREEN THE VISION



## 1.1. THE VISION FOR MONARCH GREEN

*The development at Monarch Green will signal a new chapter in the development of Redditch. It will be underpinned by the creation of a **generous green infrastructure** which retains existing green capital and incorporates a **connected network** of sustainable transport routes and spaces. A truly **distinctive** place will be formed through the retention of important site features and the aspiration to create an exciting **new garden neighbourhood** for the future.*



## GENEROUS GREEN INFRASTRUCTURE

Regard existing green capital on site as an important asset to structure the proposals. This should be maintained within generously proportioned green spaces to form the spine of the development.

Provision of multi-functional green areas that are arranged to provide a legible and meaningful structure.

Fully connected green spaces which link existing assets on and off site with new amenity spaces.

View the setting of Spring Brook as a unique and attractive asset. The creation of Spring Brook Walk will form a major new countryside edge, providing a linear space for recreation, wildlife habitat creation and play.



## CONNECTED

Providing the opportunity for transport choice; making sustainable travel attractive and convenient.

Form a connected network of green and urban connections within the site and to wider destinations. The proposals should respond to key desire lines to Monarchs Way and Sustrans Route 5.

Promote connections to the wealth of facilities located in Redditch town centre.

Encourage and enhance social connections between the new and existing community.



## DISTINCTIVE

Re-define the quality of the northern suburban interface of Redditch with the countryside.

Create a development which is inherently distinctive due to the incorporation of major existing green infrastructure and mature planting.

Celebrate the integration of development with the landscape through the retention and enhancement of visual linkages and responding to the topography.

Ensure a generosity of space within green space and the development as a whole; setting the site apart from other new developments and setting a landmark for creating a garden neighbourhood for the future.



## A NEW GARDEN NEIGHBOURHOOD

The proposals will reflect the aims and aspirations of RCBD 1.1 policy.

Create a new walkable neighbourhood where key facilities are located on the doorstep.

Provide a new local centre to aid the creation of a thriving and happy neighbourhood that encourages social interaction and community pride.

Ensure a generosity of space within the development, setting it apart from other new developments and aiding the creation of a landmark new garden neighbourhood.

## 1.2. THE DAS – STRUCTURE AND PROCESS

*This Design and Access Statement (DAS) has been prepared by Barton Willmore on behalf of the applicants. It has been written in conjunction with Bilfinger GVA, Phil Jones Associates, Wardell Armstrong and Barton Willmore Landscape. This document supports a Hybrid Planning Application for mixed use development at Monarch Green, Webheath (also known as land at Foxlydiat Lane).*

### AIMS AND OBJECTIVES OF THE DAS

1.2.1. The DAS sets out and justifies the design rationale for the proposals. It has been prepared in accordance with guidance and policy contained in the following documents;

- » Guidance on Information Requirements and Validation (CLG, March 2012)
- » Development Management Procedure Order (DPMO) (2015)
- » Streamlining the Planning Application Process: Consultation (CLG, 2013)
- » Planning Practice Guidance (2014)

1.2.2. The document is part of a comprehensive package of information submitted with the application and provides a detailed framework of guidance and specification to guide and control the elements of the development. It will explain the proposals in a concise and structured way to ensure that those considering the application will understand the rationale that underpins it.

### DAS STRUCTURE

1.2.3. The CABE document ‘Design and Access Statements – how to write, read and use them’ (2006) refers to an ‘assessment-involvement-evaluation-design process’ which it states, closely mirrors a good design process and enables the final design proposals to be informed by a clear understanding of the site and the wider context.

1.2.4. The DAS is structured in accordance with this CABE guidance and sets out the following;

#### *Outline Planning Application*

- » Presentation of parameter plans and associated wording that are to be ‘fixed’ as part of the outline planning application.

#### *Assessment*

- » An analysis of the wider context.
- » Key design influences within Webheath, Redditch and the wider area.
- » Site assessment.

#### *Involvement and Evaluation*

- » A summary of stakeholder and community involvement.
- » Evaluation based on assessment and involvement stages to inform a design concept and strategies.

#### *Design*

- » Illustrative strategy plans which help to further explain the proposals.

1.2.5. As per regulation 8(3) of the DMPO the DAS shall;

(c) Explain the policy adopted as to access, and how policies relating to access in relevant local development documents have been taken in to account.

(d) State what, if any, consultation has been undertaken on issues relating to access to the development and what account has been taken of the outcomes of any such consultation; and

(e) Explain how any specific issues which might affect access to the development have been addressed.





 Site Boundary



## 1.3. THE SITE

*The site is located within Worcestershire on the western edge of Redditch, approximately 3km west of the town centre. It sits across the administrative boundaries of Redditch Borough Council and Bromsgrove District Council.*

1.3.1. Bromsgrove is located approximately 6.5km to the north west of the site, whereas central Birmingham lies approximately 20km to the north.

1.3.2. The ownership area (edged red) extends to approximately 136.03 hectares of agricultural land which falls under the control of the applicants.

1.3.3. The south eastern edge of the site is defined by the residential area of Webheath. Further north the boundary is formed the A448 dual carriageway. By contrast much of the southern and western boundary is more rural in character, defined by the hedgerow-lined Cur Lane, Gypsy Lane and the Spring Brook.

1.3.4. Undulating rolling landform gives real variety across the site and creates a clear attribute as it rises 61m from its lowest point at the south western 'tip' near the Spring Brook to its highest point on the northern boundary near Holyoake's Farm and the A448. A network of field boundaries creates a visually contained landscape across much of the site, although the higher ground to the north is more open with long range views to the west and south. The rising ground to the east, where the site adjoins the settlement of Webheath, has a greater degree of visual containment that results from the smaller fields, greater number of hedgerows and mature trees.

1.3.5. The Monarch's Way footpath runs directly adjacent to and between the site as it passes along the south eastern boundary and Cur Lane. Elsewhere, other public rights of way can be found within the site near the southern boundary of Pumphouse Lane and the western boundary of Cur Lane. A Bridleway also crosses the site connecting between Cur Lane and Birchfield Lane.

### IEWS FROM SOUTH TO NORTH

1.3.6. The view from south to north shows the site within its context of the residential area of Webheath to the east and the wider urban area of Redditch beyond. The well defined and established landscape structure found in the southern part of the site is prominent in this image, particularly along the Spring Brook which forms the western boundary.

### IEWS FROM NORTH TO SOUTH

1.3.7. The view from north to south shows how the fields located in the northern part of the site are more expansive than those found towards the south. The A448 dual carriageway is a dominant feature along the north eastern boundary, whereas the lower part of Gypsy Lane is also visible. Again, the residential area of Webheath can be seen adjoining the site on its eastern side.





Site Boundary

Gypsy Lane

The Site

A448

Cur Lane

Foxlydiate Lane

Webheath

Pumphouse Lane





View of the site from south to north (site boundary excluding highways works)







## 1.4. PROPOSAL OVERVIEW

*The proposals for Monarch Green will deliver an attractive, connected and distinctive new garden neighbourhood where people aspire to live.*

The Hybrid application will comprise:

### 1) OUTLINE APPLICATION

Outline application (all matters reserved with the exception of vehicular points of access and principle routes within the site) for the demolition of existing buildings and the erection of:

- » Up to 2,800 dwellings (Class C3)
- » Local centre including retail floorspace up to 900 sq metres (Classes A1, A2, A3) health and community facilities of up to 900 sq metres (Class D1)
- » A 3FE first school (Class D1) (up to 2.8Ha site area) including associated playing area and parking.

#### *Associated provision of:*

- » Roads, car parking, cycleways and footpaths
- » Public open space including formal play area, informal incidental play space, children's play areas including multi-use games areas (MUGA)

- » Landscaping / buffer areas
- » Ecological mitigation measures
- » Means of enclosure / boundary treatments
- » Sustainable drainage systems, including flood alleviation works
- » Associated works including earthworks, remediation, utilities, connections and ancillary structures, street lighting and tree removal
- » Creation of new access points at Birchfield Road, Foxlydiate Lane / Cur Lane junction, and Pumphouse Lane.

### 2) DETAILED APPLICATION

Detailed application for the creation of a means of access off Birchfield Road, Cur Lane, Foxlydiate Lane and Pumphouse Lane. The creation of a primary access road, including associated cut and fill works and other associated earthworks, landscaping, surface water drainage and attenuation.





-  Primary Vehicular Access Point
-  Secondary Vehicular Access Point
-  Residential Development
-  Local Centre
-  Primary School
-  Public Open Space
-  Existing Tree/Hedges/ow Planting
-  Proposed Tree/Hedges/ow Planting
-  Area for Formal Play
-  Allotments/Grow Patches
-  Areas for attenuation/drainage
-  Brailleway
-  Public Right of Way
-  Monarch's Way
-  Proposed Cycle Route
-  Proposed Informal Pedestrian Route
-  Possible future development within allocation area







*Chapter 2:*

# OUTLINE PLANNING APPLICATION PROPOSALS

## 2.1. SUMMARY AND STATUS OF THE PROPOSALS

*The parameter plans and associated wording in this chapter are to be ‘fixed’ as part of the outline planning permission and they provide a framework for future, more detailed designs. This approach is illustrated on the masterplan cascade diagram, presented opposite.*

2.1.1. In order to provide as much helpful design information as possible at each appropriate stage a phased, cascade of plans is proposed. This allows for flexibility and adaptability so that the design of each phase can respond to issues important at a particular time or in a particular location on the site, but held within a consistent parameter approach. Importantly, the cascade sets a framework to provide for future design fixes at an appropriate time, as referred to in this chapter.

2.1.2. In addition to this detailed information for access, surface water drainage and landscape (as per the description of development) will be provided as part of the planning application. These proposals sit outside of the approval structure identified above. Full details of these proposals will be provided within the application package and thus, all information contained in this DAS must be cross referenced to these additional detailed plans.

### DESIGN INFORMATION SUBMITTED AS PART OF THIS OUTLINE APPLICATION

2.1.3. The following elements form part of this application;

#### *Parameter Plans*

2.1.4. The parameter plans are fixed and for which planning permission is sought. They will provide information for;

- » Land Use
- » Access and Movement
- » Building Heights
- » Green Infrastructure: Planting Retention and Removal

#### *Supporting Design Strategies*

2.1.5. The parameter plans will be supported by a set of design strategies that are presented in chapters 7 – 9 of the DAS. These plans do not form part of the outline approval, they are illustrative and should be read in conjunction with the parameter plans. The LPA may wish to request that some of these plans become approved as part of the outline permission, in agreement with the applicants. They are:

- » Density
- » Street Hierarchy
- » Foot, Bus and Bike
- » Legibility Framework
- » Play Strategy
- » Residential Phasing

### DESIGN MATERIAL TO BE SUBMITTED POST APPROVAL OF THE OUTLINE PERMISSION IN ACCORDANCE WITH THE CONDITIONS

2.1.6. The following material will be prepared subsequent to the submission and approval of the outline planning permission. It will build on the approved parameters and subsequent principles included in the illustrative design chapters of the DAS to show how a high quality place can be created that delivers the key vision objectives and design principles.

» **Framework Plans.** They will consist of a 1:1000 illustrative plan showing indicative locations for plots and buildings and will be prepared for each phase of development. These plans will cover approximately 500 dwellings and will illustrate how the design principles in the DAS will be applied at a more detailed level.

» **A design guide or appearance palette.** This short document will set out information on the proposed appearance of the area covered by the 1:1000 Framework Plan. It will provide principles for architecture, materials, landscaping and parking and references the principles presented within the DAS.

» All of the above will be required (by condition) to be submitted prior to the submission of reserved matters applications.

## MASTERPLAN CASCADE





## 2.2. LAND USE

*The Land Use Parameter Plan presented here defines the extent of the proposed land uses.*

### RESIDENTIAL

2.2.1. The proposals provide 75.80ha for residential development (excluding land for the local centre and education uses) and will deliver up to 2,800 dwellings in a range of types and tenures.

2.2.2. The development proposes a phased approach; this is set out in section 7.7.

### LOCAL CENTRE

2.2.3. A mixed use local centre of 0.62ha will be provided. It will include retail, health, community and residential uses.

2.2.4. Maximum floorspace amounts for the mixed use centre are set out in the table below;

Use	Maximum Floorspace (m <sup>2</sup> )
Retail (Uses A1, A2, A3)	900
Health (Uses D1)	650
Community (Uses D1)	250

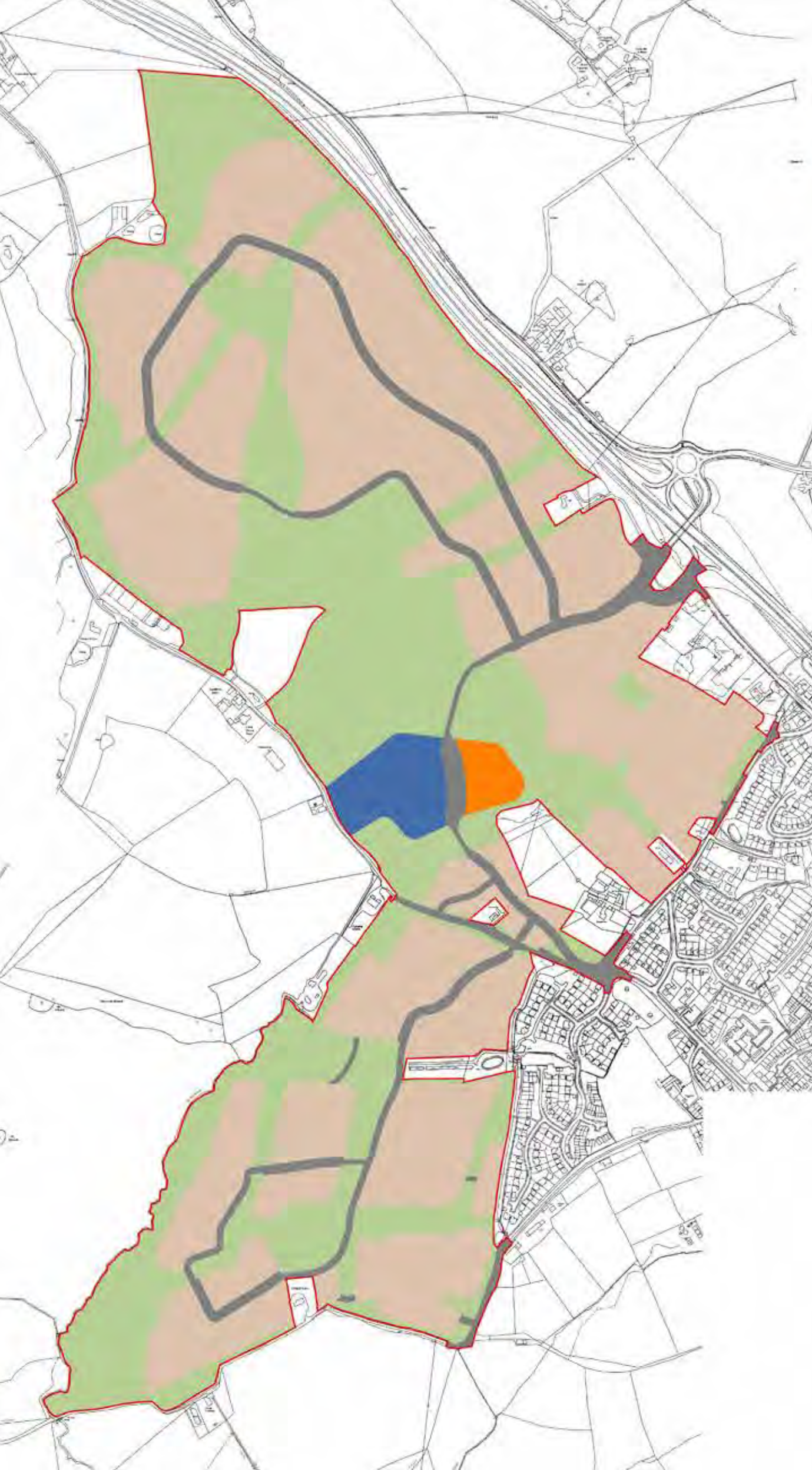
### EDUCATION

2.2.5. The development will provide a three form entry (3FE) first school which is located on a 2.8ha site.

### PUBLIC OPEN SPACE

2.2.6. 53.23ha of public open space will be provided to include informal and formal open space, drainage, existing and proposed structural planting, drainage and play. Sustainable Urban Drainage Systems (SUDS) will be provided within areas of public open space.





- Site Boundary
- Residential
- Education
- Mixed Use (to include residential, A1, A2, A3 and D1)
- Infrastructure (to include associated off-site highways works)
- Public Open Space (to include existing structural planting, new structural planting, drainage and outdoor leisure facilities)

## 2.3. ACCESS AND MOVEMENT

*The Access and Movement Parameter Plan presented here identifies locations for vehicular access points in addition to strategic vehicle connections and existing footpath routes.*

### VEHICULAR ACCESS POINTS

2.3.1. Vehicular access to the proposed development will be taken from the following points;

- » Birchfield Road
- » Foxlydiat Lane
- » Cur Lane
- » Pumphouse Lane

2.3.2. The primary access will be taken from Birchfield Road, where it meets A448 Bromsgrove Highway. The grade separated junction will be modified to accommodate a new signal controlled junction.

2.3.3. The current alignment of Cur Lane from the Pumphouse Lane roundabout will be closed and replaced by a new road that will connect through the site to Birchfield Road. Cur Lane north of the Severn Trent pumping stations, will be reconnected into the new road via one of the estate roads.

### PRIMARY AND SECONDARY VEHICULAR MOVEMENT ROUTES

2.3.4. The Primary vehicular route through the site will be between Cur Lane and Birchfield Road. This will both provide access to the whole development, but also provide an alternative route for existing residents from Webheath to the A448. A small number

of properties may take direct access from this road, however this will be designed as a boulevard with landscaped areas including street trees, walk and cycle routes and some on-street car parking.

2.3.5. The Secondary vehicular routes through the site will be accessed off the primary route and provide a route to the various residential parcels of the new development. They will take direct access to properties. Pavements will be provided on either side, with some shared cycle routes and landscaping including street trees.

### PEDESTRIAN AND CYCLE MOVEMENT

2.3.6. Existing pedestrian and cycle access points to the site will be retained, along with the route of the bridleway and the existing Public Right of Way (PRoW).

2.3.7. Pedestrian and cycle access to the development will be available from Pumphouse Lane, Cur Lane, Foxlydiat Lane and Birchfield Road. Pedestrians and cyclists will be provided with a network of permeable and direct route, that connect the site with neighbouring communities and Redditch Town Centre. The National Cycle Network (Sustrans Route 5) passes through Webheath and there is an opportunity to create a high quality route through the development, including some off road segregated cycle paths. The strategy for pedestrian, cycle and bus movement is set out in section 7.2.





- Site Boundary
- Primary Access Point
- Secondary Access Point
- Primary Vehicular Route (along the alignment of +/- 20m)
- Secondary Vehicular Route (along the alignment of +/- 20m)
- PROW
- Other paths (Monarch's Way)

Access and Movement  
Parameter plan

## 2.4. SCALE

*The range of building heights illustrated on the Parameter Plan presented opposite have been established in response to a combination of factors including site and local context assessment, topography, proposed densities and best practice urban design principles.*

2.4.1. The heights shown are maximum building heights that will be assessed as part of the Environmental Statement. It is intended that variation will happen within these maximum building heights.

2.4.2. Storey heights will vary between 2 and 3 storeys, with the majority of development consisting of 2 and 2.5 storeys. Variations in building scale throughout the development will aid legibility and the creation of a distinct identity for the proposals.

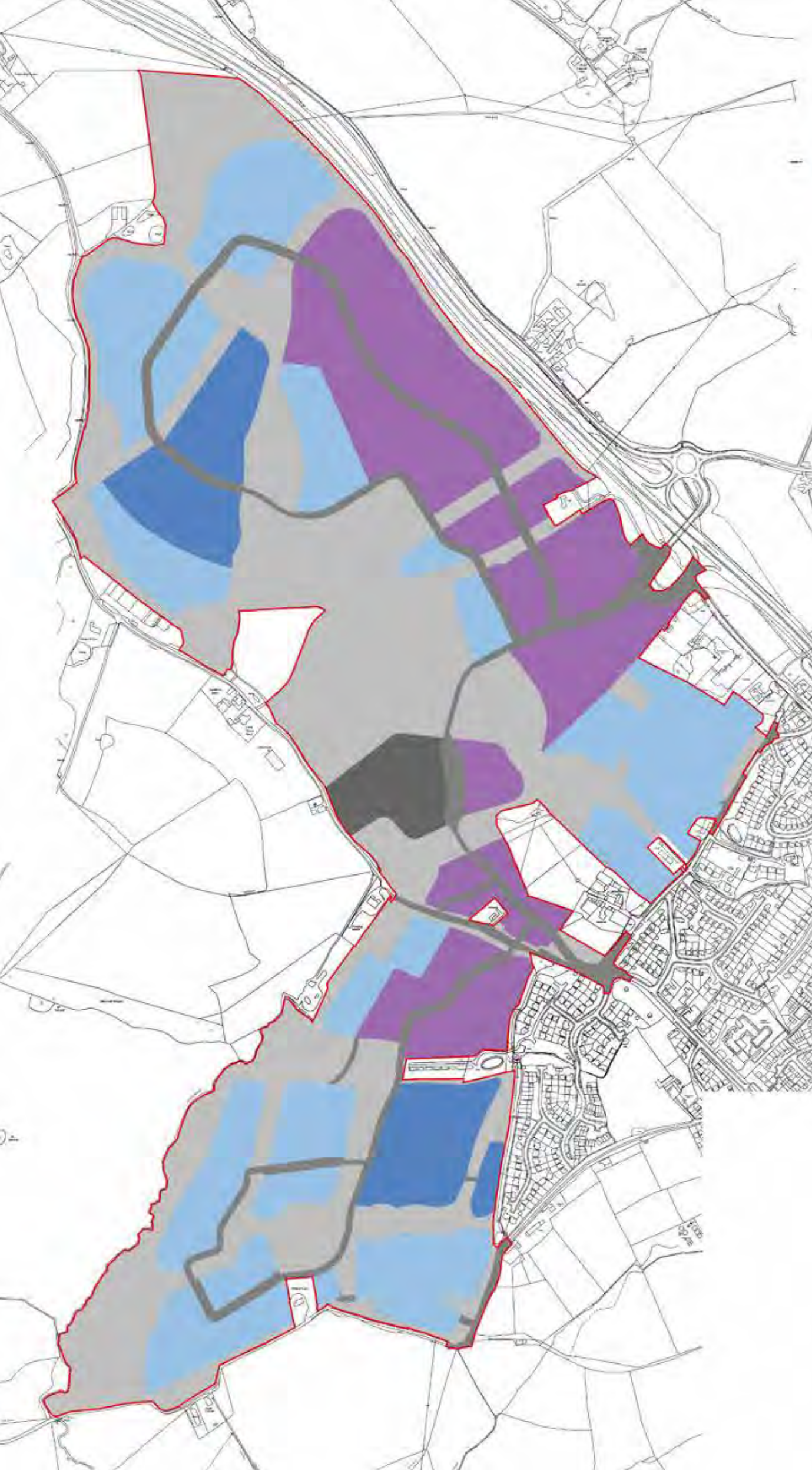
2.4.3. Areas of 2 storey development will be located in the following areas;

- » The most northern area and south / south-western edge of the site, responding to the topography and helping to provide a sensitive transition between open space and the proposed built form.

- » The central parkland frontage, respecting the setting of Lane House Farm.
- » Adjoining Foxlydiate Lane to respect the amenity and privacy of existing dwellings that front on to the site boundary.

2.4.4. Building heights of up to 2.5 and 3 storeys will be used where it would be beneficial to emphasise certain locations such as the local centre, the primary movement route, a key building or public open space, aiding legibility and a sense of enclosure.





- Site Boundary
  - Up to 2 Storeys  
approximately 9m ridge height
  - Up to 2.5 Storeys  
approximately 11m ridge height
  - Up to 3 Storeys  
approximately 12m ridge height
- Key buildings to be upto 3 storeys where indicated within legibility plan

## 2.5. GREEN INFRASTRUCTURE: PLANTING RETENTION AND REMOVAL

*The Green Infrastructure Parameter Plan, presented opposite shows areas of existing tree and hedgerow planting that will be retained and areas that are proposed for removal.*

2.5.1. The masterplan has retained a significant amount of existing tree and hedgerow planting located within the site and used this to shape areas of development. This existing planting will be retained, enhanced and improved. New planting located within streets and areas of public open space will bring further benefit to the site.

2.5.2. Strategies for green infrastructure, play and drainage are set out in section 7.





- Site Boundary
- Trees/hedgerows to be retained
- Trees/hedgerows to be removed
- Public Open Space  
(to include existing structural planting, new structural planting, drainage and outdoor leisure facilities)





*Chapter 3:*

# ASSESSING THE CONTEXT: STRATEGIC

## 3.1. PLANNING CONTEXT

### REDDITCH AND BROMSGROVE LOCAL PLAN

3.1.1. Both Redditch Borough Council and Bromsgrove District Council have prepared Local Plans which are currently undergoing an Examination in Public. These plans seek to meet the development and housing needs of the area up to 2030. Redditch Borough Council identified a housing need of approximately 6,400 dwellings within the plan period to 2030, however the Council can only demonstrate capacity for 3,000 dwellings within Redditch Borough, leaving a shortfall of approximately 3,400 dwellings.

3.1.2. In order to meet the housing needs of Redditch, the two Council's have agreed that the Bromsgrove District Plan (BDP) should allocate two sites under Policy RCBD1:

- » Foxlydiat – for a minimum of 2,800 dwellings (and supporting facilities) – Policy RCBD1.6 and 1.7); and
- » Brockhill – for a minimum of 600 dwellings (and supporting facilities) – Policy RCBD1.6 and 1.8).

3.1.3. Redditch Borough Council's Local Plan No.4 and the Bromsgrove District Plan set out a vision for the Foxlydiat site in the emerging Policy RCBD1 Redditch Cross Boundary Development, which states that development will include a minimum of 2,800 dwellings, a first school and a local centre, including associated community infrastructure.

3.1.4. The Policy wording is set out below;

### FIGURE 2.1 RCBD1 (INCLUDING MINOR CHANGES, FEB 2014)

#### ***RCBD1.1 Redditch Cross Boundary Development***

3.1.5. RCBD1.6 Two mixed use urban extensions are proposed (as shown on Map 10 RCBD1 page 44) across two sites adjacent to Redditch and are appropriate to deliver approximately 3400 dwellings and comprehensive provision of associated new infrastructure to meet some of Redditch's housing requirements up to 2030.

3.1.6. RCBD1.7 Site 1 Foxlydiat will include approximately 2800 dwellings, a first school and a Local Centre, including associated community infrastructure.

3.1.7. RCBD1.8 Site 2 Brockhill will contain approximately 600 dwellings which will integrate with the Strategic Site at Brockhill East, as shown in the Redditch Local Plan No.4 and should integrate well into the existing urban fabric of Redditch.

3.1.8. RCBD1.9 In order to achieve these sustainable new communities all aspects of the delivery of the urban extensions must be in accordance with the Policies contained within the Bromsgrove Development Plan and any other relevant Policies. In addition, it is a requirement that the following principles are applied to both sites:

3.1.9. I. The residential development will reflect the local requirements as detailed in the most up-to-date Housing Market Assessment and comprise of up to 40% affordable housing with a flexible mix of house types and tenures.

3.1.10. II. An overall Transport Assessment will be produced taking into account the cumulative and wide ranging effects of development on transport infrastructure including new and improved access arrangements, which are in keeping with the structured road hierarchy.

3.1.11. III. Significant improvements in passenger transport will be required resulting in integrated and regular bus services connecting both sites to key local facilities. In particular, services should be routed through both Site 1 Foxlydiat and Site 2 Brockhill, with all dwellings to be located within 250m of a bus stop.

3.1.12. IV. Walking and cycling routes should be well integrated with the Green Infrastructure Network. Site 1 Foxlydiat should make full use of existing walking and cycling routes, such as Sustrans Route No. 5 and Monarch's Way and Site 2 Brockhill should create routes.

3.1.13. V. Both sites will have an overall Strategy and Management Plan for Green Infrastructure which maximises opportunities for biodiversity and recreation, whilst protecting existing biodiversity habitats and landscape



geodiversity. Green Corridors should be created around Spring Brook in Site 1 Foxlydiate and the Red Ditch in Site 2 Brockhill. Both sites should be sensitively designed to integrate with the surrounding existing environment and landscape. In particular, development should be respectful and sympathetic to the topography of the sites, with no development on prominent ridge lines and where appropriate retain tree lined boundaries.

*3.1.14.* VI. Flood risk from the Spring Brook on Site 1 Foxlydiate and the Red Ditch on Site 2 Brockhill East should be managed through measures that work with natural processes to improve the local water environment. Any necessary measures to mitigate flood risk are to be implemented and flood modeling will be required, which must be outlined in a site specific Flood Risk Assessment. Surface water runoff must be managed to prevent flooding on, around and downstream of both sites through the use of Sustainable Drainage Systems (SuDS).

*3.1.15.* VII. SuDS proposals on Site 1 must provide an appropriate level of treatment to avoid pollution risks to controlled waters, and be designed to achieve the greenfield rate of run-off, maximise recharge to the underlying aquifer and support water levels in the Bow Brook. In accordance with the

objectives of the Water Framework Directive, development should ideally contribute towards the improvement of, but as a minimum not have a deteriorative effect on, the water bodies associated with the site.

*3.1.16.* VIII. An appropriate assessment of the pollution risks to controlled waters on Site 1 Foxlydiate will be produced taking account of any previous contaminative uses on the site including the historic landfill, and the risks associated with the proposed uses

*3.1.17.* IX. Proposals for development will need to ensure that sufficient capacity of the sewerage systems for both wastewater collection and treatment is provided through engagement with Severn Trent Water Ltd and the Environment Agency and delivered at the appropriate stage.

*3.1.18.* X. All development must be of a high quality design and locally distinctive to its surrounding rural and urban character; contribute to the areas' identity and create a coherent sense of place; and respect and enhance the setting of any heritage asset. There should be a continuous network of streets and spaces, including the provision of public open spaces, creating a permeable layout with well-defined streets.

*3.1.19.* Xi. In preparing development proposals, provision should be made for any necessary infrastructure for the effective delivery of the site.

*3.1.20.* Xii. Any proposals for development on either site must not individually or cumulatively jeopardise the future use of any other part of the site (s) or impede the delivery of the two sustainable communities.

## EXAMINATION IN PUBLIC

*3.1.21.* The Examination of the Local Plan is continuing, and the outcome will not be known until the Inspector releases his decision. Given the scale of the site and inputs necessary in order to ensure phased delivery of development in accordance with the trajectory set out within the emerging BORLP4, the Hybrid Planning Application is being brought forward now to ensure that determination can take place at the earliest available time.

## 3.2. ACCESS AND MOVEMENT

### LOCAL HIGHWAY NETWORK

3.2.1. The site is very well located with access onto the A448, which provides direct routes to Redditch, Bromsgrove, Birmingham and the Strategic Highway Network. The A448 Bromsgrove Highway is a 60mph dual carriageway road which borders the site to the north.

3.2.2. Foxlydiate Lane, Church Road, Birchfield Road and Heathfield Road are local roads which serve to the site, which are subject to a 30mph speed limit and provide access to neighbouring local rural properties and the local communities in Webheath and Batchley. These roads provide connectivity to local amenities and neighbouring communities.

3.2.3. Cur Lane and Pumphouse Lane to the south provide access to rural properties and local farms and are subject to the national speed limit.

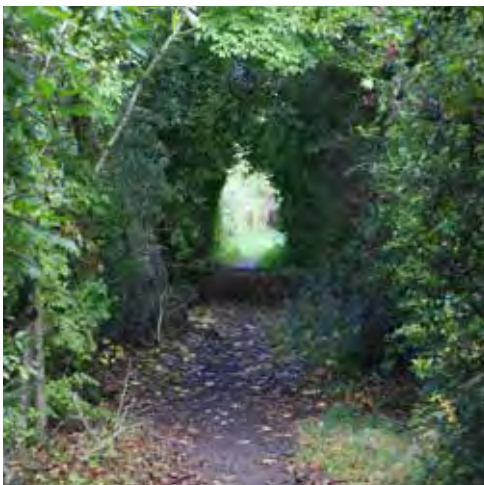
### SUSTAINABLE TRAVEL

#### *Walking*

3.2.4. Foxlydiate Lane and Birchfield Road have footways on one side of the road along their entirety, except on the approach to major junctions where there are typically footways on both sides. These routes connect the local community and provide access to local amenities and bus stops.

3.2.5. The site benefits from a number of Public Rights of Ways (PRoW) and a Bridle path which are located within the site boundary. This includes Monarch's Way which is a walk of historical significance. These PRoW's provide excellent recreational opportunities for future residents of the site.

3.2.6. The Webheath estate is a permeable network of quiet streets, which provide a good environment for walking and cycling. They provide good community links and routes to bus stops and local amenities.



*The Monarchs Way adjoins the south eastern site boundary*

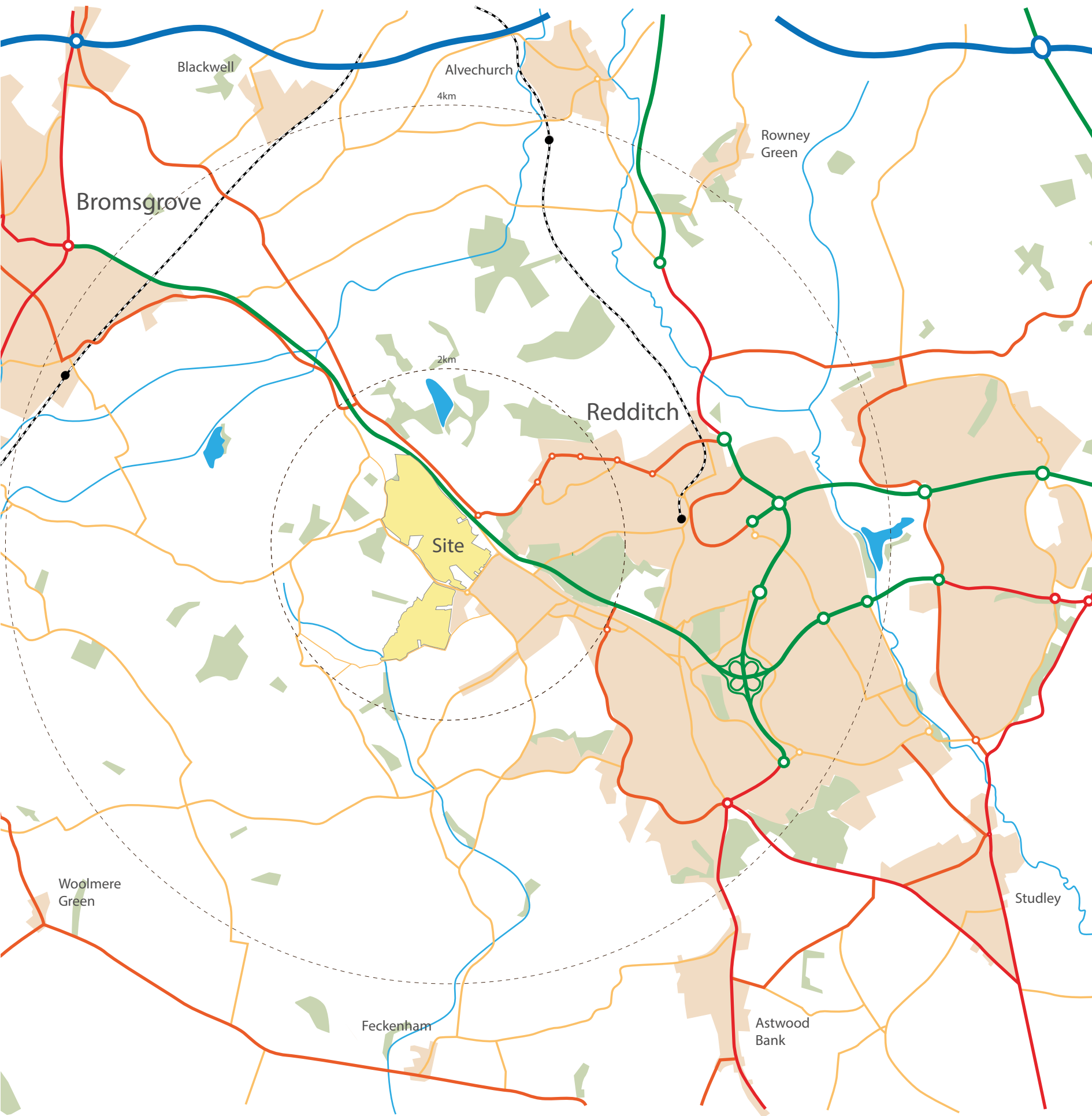


*Cycling route located on Church Road*





*The A448 adjoins the north eastern boundary of the site*





Strategic Transport and Movement plan

- |   |                          |   |                 |
|---|--------------------------|---|-----------------|
|  | Site                     |  | B Roads         |
|  | Developed Areas          |  | Other Key Roads |
|  | Dual Carriageway A Roads |  | Railway Line    |
|  | A Roads                  |  | Railway Station |

### **Cycling**

3.2.7. The site can be accessed directly from National Cycle Route 5, which provides direct connectivity into Redditch and Bromsgrove.

3.2.8. Redditch Railway Station, a GP Practice and a Secondary School can be reached within a 10 minute cycle journey from Webheath.

### **Bus**

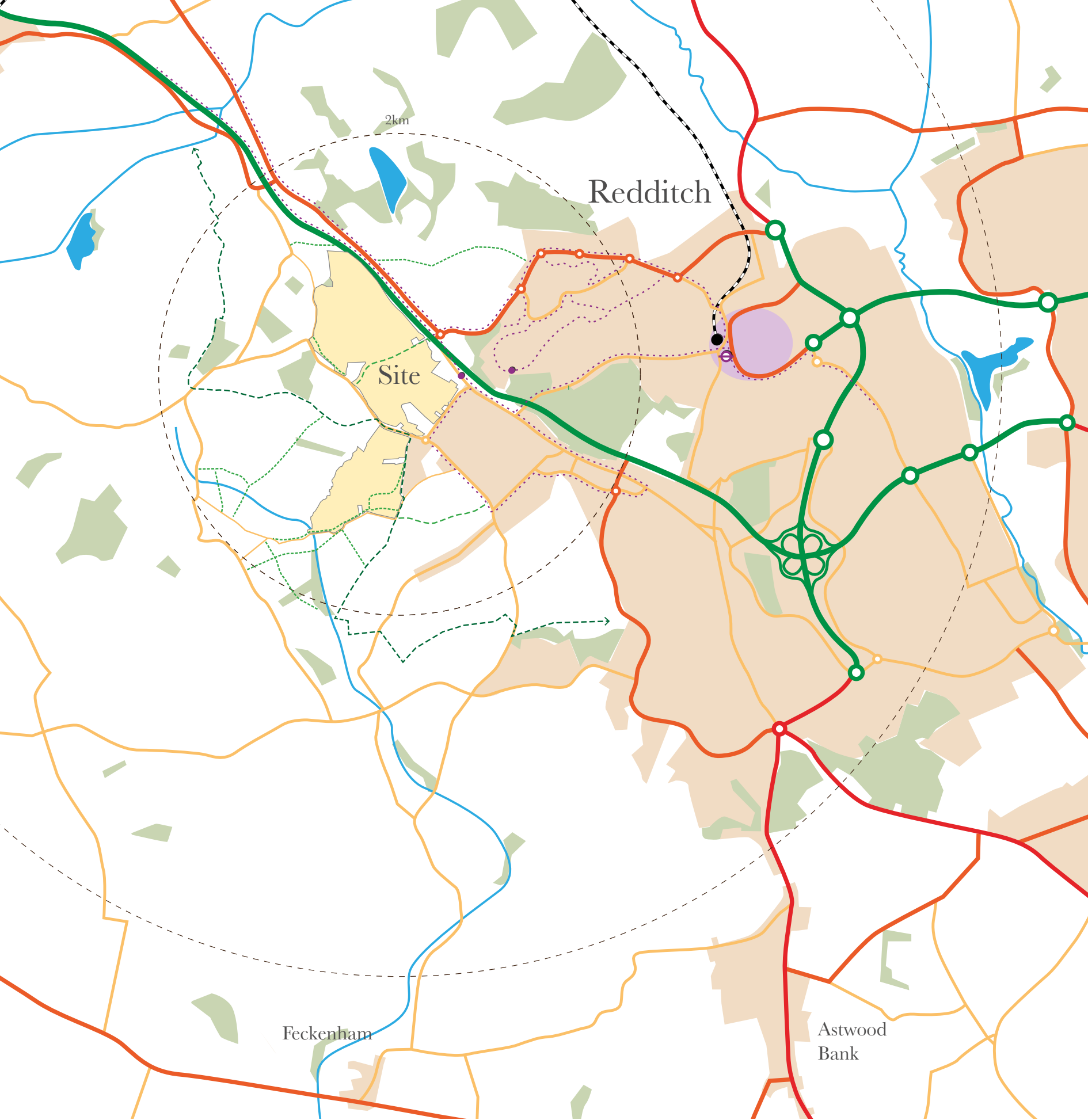
3.2.9. Local routes combine to provide 15 buses per hour providing access to Redditch Town Centre, Redditch (other areas), Bromsgrove, and Alexandra Hospital. Local stops are located on Birchfield Road and Church Road.



*On-road national cycle route on Bromsgrove Road*

Bus Number	Destination	Weekday	Saturday	Sunday
47/48	Redditch town centre	2	2	1
	Alexandra Hospital	2	2	1
51	Redditch town centre	6	6	2
53	Redditch town centre: first arrival	1	1	-
143	Redditch town centre	1	1	-
	Bromsgrove and Aston Fields	1	1	-
X3	Bromsgrove	1	1	3 hour interval
	Kidderminster	1	1	-

*Summary of Bus Services*



Local Transport and Movement plan

- |                          |                 |                        |                      |
|--------------------------|-----------------|------------------------|----------------------|
| Site                     | A Roads         | Bus Route              | Footpath             |
| Developed Areas          | B Roads         | Bus Stop               | Redditch Town Centre |
| Dual Carriageway A Roads | Other Key Roads | Bus Station            | Woodland             |
| A Roads                  | Railway Line    | Monarch's Way Footpath | Watercourse          |
| B Roads                  | Railway Station | Bridleway              |                      |



## 3.3. LOCAL FACILITIES

### EDUCATION

3.3.1. Two first schools serve the Webheath estate, including Our Lady of Mount Carmel Catholic School and Webheath First School. These are a 15 minute walk from the eastern site boundary of the site through quiet residential streets.

3.3.2. Middle schools are found in Batchley and Headless Cross, which are 10 minutes by bicycle. There are three High schools within a 15 minute cycle ride, including Trinity High School and Sixth Form College, Saint Augustine's Catholic school and Woodfield Academy.

### HEALTHCARE

3.3.3. Nearby GP services are provided on Bromsgrove Road, in Headless Cross and in Redditch Town Centre. Dental services are provided nearby in Webheath. Alexandra Hospital is a 25 minute bus ride away on Route 47, and a 20 minute cycle ride for employees.

### LOCAL SHOPS

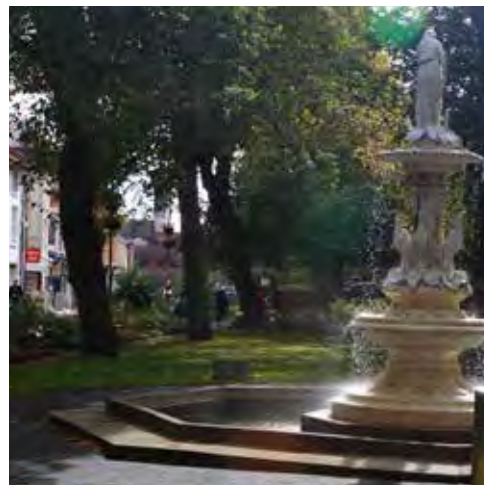
3.3.4. Foxlydiate Lane is less than two miles from Redditch Town Centre, which is a 35 minute walk and an 11 minute cycle ride. The Town Centre provides a comprehensive mix of convenience and food retail offers. More locally on Heathfield Road there is a Post Office and Convenience Store (a 15 minute walk). Tardebigge Court, near Hewell Grange, which is a 10 minute cycle ride away, includes a café and salon.

### EMPLOYMENT AREAS

3.3.5. The main employment areas for local residents are found in Redditch (Town Centre, Park Farm and Mappleborough Green) and Bromsgrove. These are accessed by bus routes X3 and 143, which provide bi-hourly services on Weekdays and Saturdays, and the local cycle network.



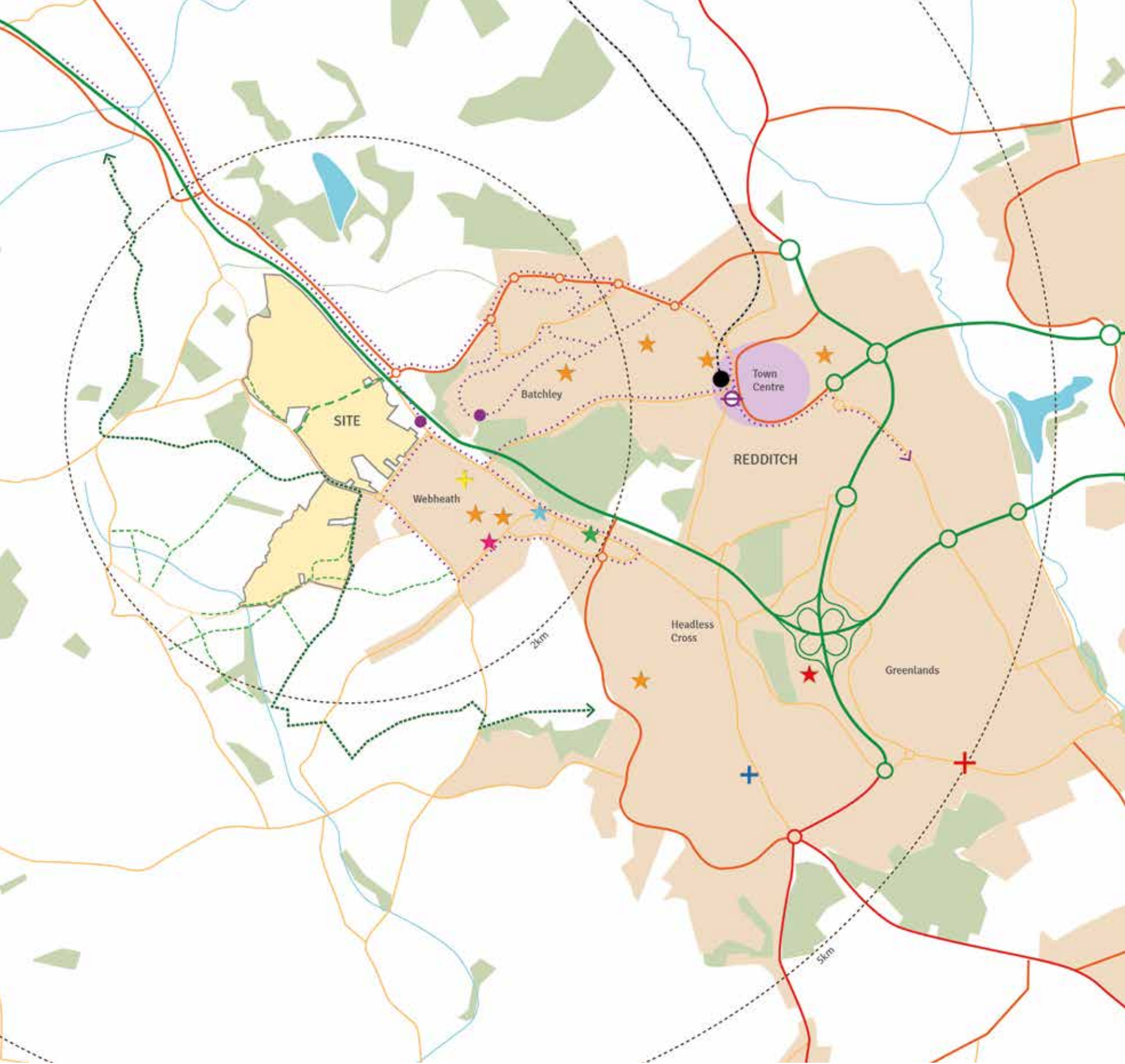
Webheath Local Centre



Formal open space in Redditch town centre



Webheath Village Hall



Local Facilities plan



## 3.4. LANDSCAPE BASELINE

### LANDSCAPE CHARACTER OF THE SETTING

*3.4.1.* The wider landscape setting is a description of how the site relates to the host landscape. The Worcestershire setting is an important contributor to the composition of the character of the site. Landscape character is defined as: ‘An expression of pattern, resulting from particular combinations of natural and cultural factors that make one place different from another, rather than better or worse.’ (Countryside Agency: LCA guidance).

*3.4.2.* The components of landscape character include the landform, land use, settlement pattern and its history, transport routes as well as the nature of the vegetation and tree cover. These elements and components have relevance to the character of the Site as they combine to distinguish the wider setting for the site at the edge of Redditch but within the landscape of the countryside. Both urban and rural influences exist and at the settlement edge and have been recorded as character influences within the study.

*3.4.3.* The Landscape character of the setting has been recorded in national and local landscape character Assessments. Landscape character at the national level is contained within the Natural England (NE) character map and it places the site within two National Character Areas (NCA): ‘97: Arden’ and NCA: ‘106: Severn and Avon Vales’. The boundary of the two NCAs runs for part of its length, within the site: in broad terms the land to the north west of Hawthorn Pit is within NCA ‘97: Arden’ which is the generally higher and more open land and to the south of the Hawthorn Pit the land is within NCA: ‘106: Severn and Avon Vales’.

*3.4.4.* At a local level the Worcestershire Landscape Character Assessment (WCC. 2011) describes the Local the setting to the south as Landscape Type: Principal Timbered Farmlands. The Principal Timbered Farmlands LT is a rolling lowland landscape with occasional steep sided hills and low escarpments. They have a small scale, wooded, agricultural appearance characterised by filtered views through densely scattered hedgerow trees. These are complex, in places intimate, landscapes of irregularly shaped woodlands, winding lanes and frequent wayside dwellings and farmsteads. The landscape to the north is within the Wooded Estates LT.

*3.4.5.* The setting is characterised by the farmed and settled lower lying ground around Cur Lane as well as by the rising, locally high ground towards the northern boundary with the A448 dual carriageway. Small hamlets and farm complexes are set in a farmed landscape of occasionally large fields. The landform is rarely flat or level over large areas, as it rolls and dips to water courses or rises to form low shoulders of small hills. Copses and areas of woodland, linked by strong and visually prominent hedgerows form a distinctive landscape and visual baseline framework with a pattern that is divided and frequently enclosed but also frequently open. There are occasional long views over the Vale of Evesham to a skyline made by both the Malvern Hills and the Cotswold Hills. The presence of settlement in terms of buildings, visible in the landscape, is a near constant feature but the scale of the countryside is considerable and even the large roads that are also common elements in this large scale landscape have an effect that is diminished by the expanse of the landscape.

### LANDSCAPE CHARACTER OF THE SITE

*3.4.6.* The character of the site results from its use, its range in scale, the variation in its openness and its enclosure, and from its distinctive and complex rolling and rising landform and from the character influencing presence of the copses and woodland that are a common feature. The level change across the site forms a complex pattern and the shape and orientation of the rising ground to the settlement edge of Webheath to the east and to the A448 to the north are key baseline elements that give character to the site as well as constraining and guiding development strategies.

*3.4.7.* These contributing attributes are further characterised by the site’s proximity to the urban edge the western side of Webheath that is visible from the site. This edge of Webheath is on rising ground above the land at the edge of the Site and there are numerous locations where there are lines of sight between housing in Webheath and the site.





Landscape and Visual Constraints and Opportunities Plan







Hawthorn Pit

*Public Right of Way near Hunters Hill house looking south west over the site towards Cur Lane, Spring Brook and the distant Malvern Hills*





Cotswold Hills

Cur Lane

Public Right of Way





A448 (in cutting)

Hunters Hill house

*On site looking south to Cur Lane and the edge of Webheath towards the Malvern Hills*





Webheath

Pumphouse Lane

Cotswold Hills





A448 (in cutting)

Tack Farm

Hunters Hill house

Hawthorn Pit

*On site at Cur Lane near the Spring Brook looking north to Hawthorn Pit and Hunters Hill house*





Hawthorn Pit

Spring Brook



## 3.5. VISUAL BASELINE

### VISUAL BASELINE

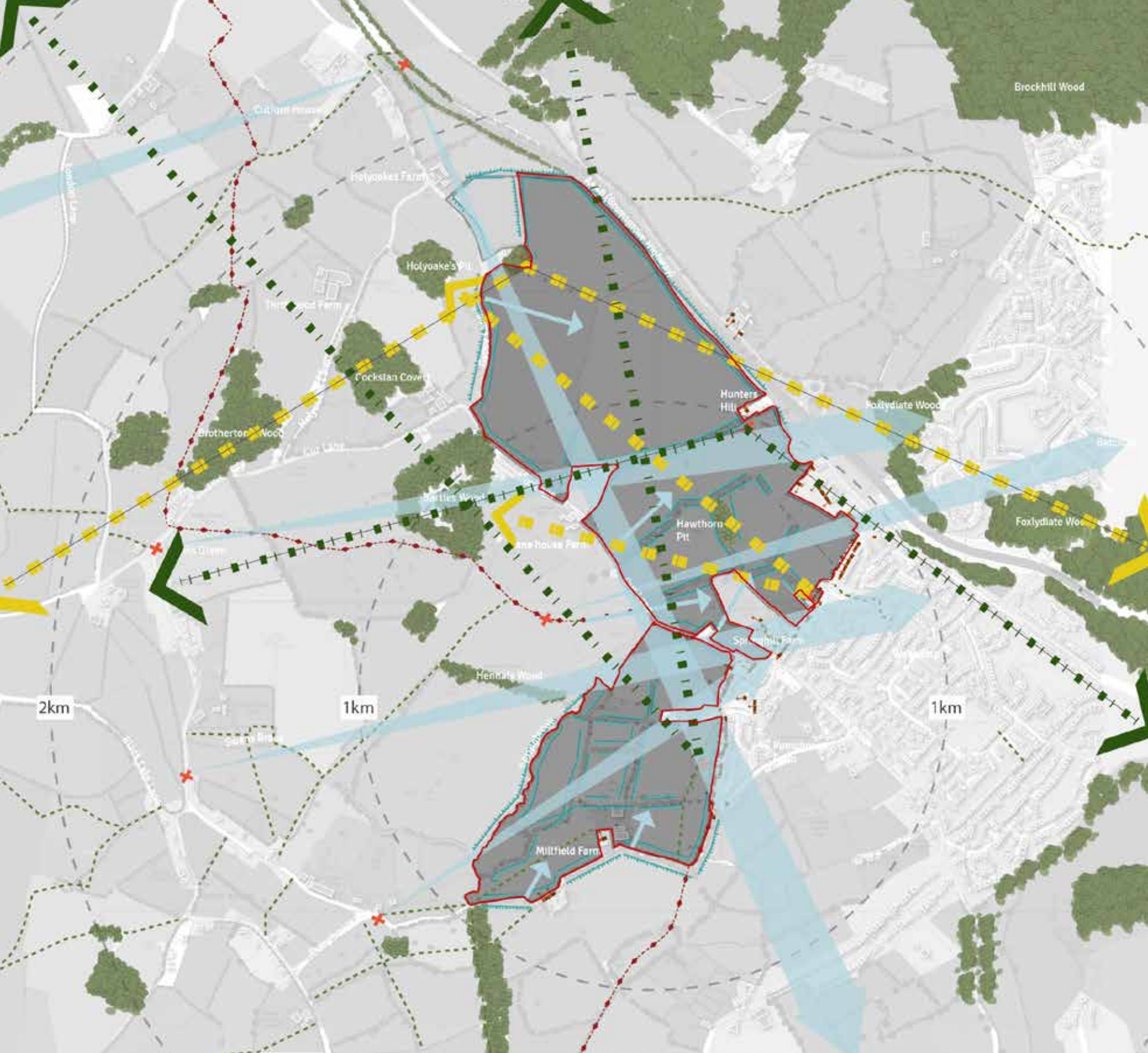
3.4.8. The PROW's that cross the site have been walked and evaluated and have been recorded. The northern area has only a few public paths and the Bridle path from Hunters Hill House provides sequential viewpoints on a westerly journey that are wide ranging and expansive and that have a westerly orientation off and over the site.

3.4.9. The southern area has a number of paths running from Pumphouse Lane. These also provide sequential viewpoints on a north and north westerly journey, that are wide ranging and expansive and that have a north westerly orientation off and over the site.

3.4.10. The lower lying lanes have hedges and hedgerow trees along significant proportions of their route. Cur Lane is enclosed by tree canopies as it drops down the hillside from Webheath and Gypsy Lane is similar in its tree cover at its northern end. The vegetation contains potential viewlines; frequently limiting viewpoints over the site to just gateways







Visual Baseline - views to and from the site









*Chapter 4:*

# ACCESSING THE CONTEXT: LOCAL



## 4.1. HISTORIC CONTEXT

*An analysis of the historic environment can provide important contextual cues for shaping the proposed development.*

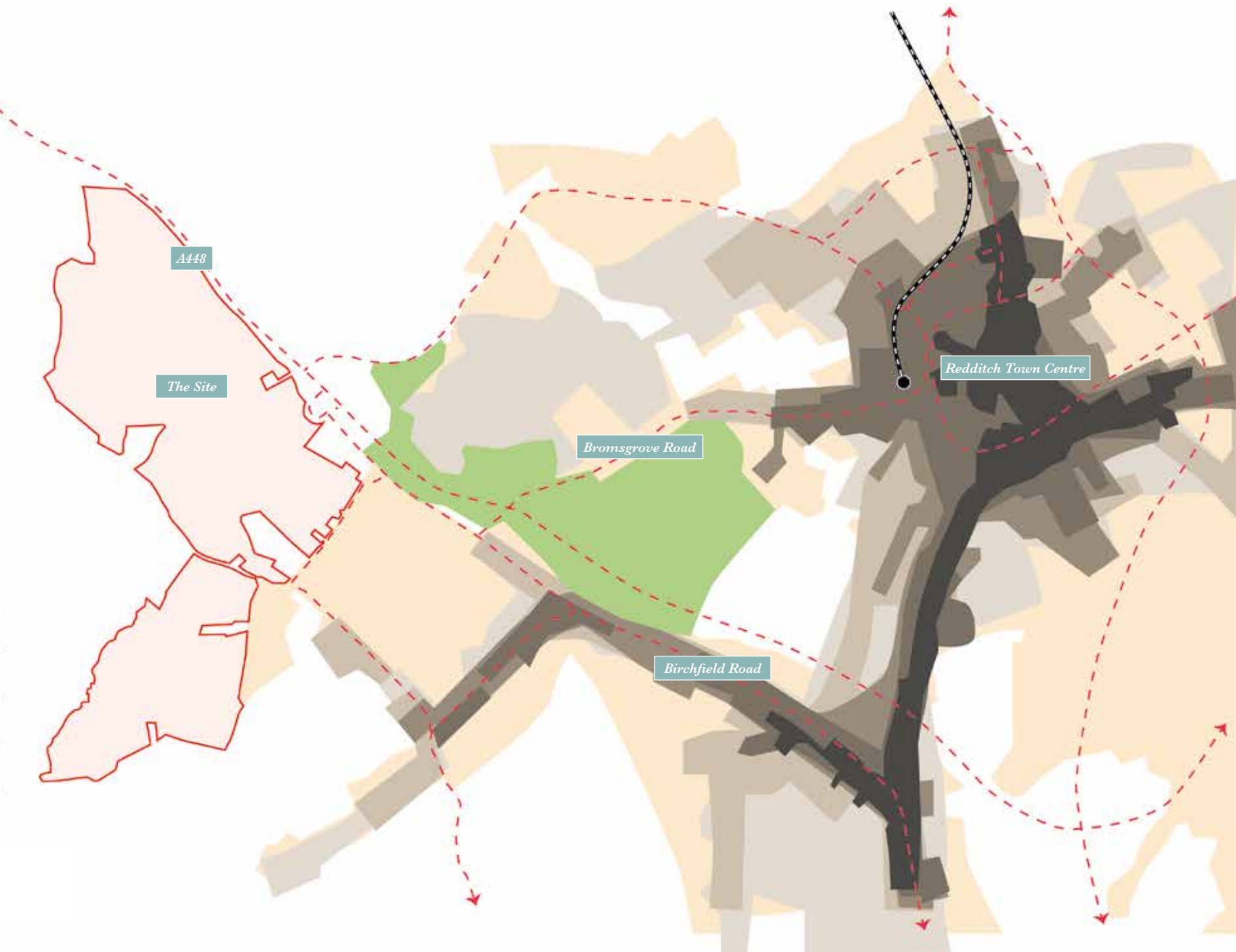
4.1.1. Following the dissolution of Bordesley Abbey in the 16th century the town of Redditch emerged, formerly part of the ancient parish of Tardebigge. The early settlement was made up of a small group of cottages located on the cross roads that can be found today at Church Green in the town centre.

4.1.2. The Parish of Redditch was founded in the mid 19th century as the town continued to grow based on prosperous local industry centred on the manufacturing of needles. The settlement expanded along the north to south route linking Birmingham to Evesham and the road west to Bromsgrove. By 1870, completion of the 'Birmingham to Gloucester Loop' introduced rail travel to the town which coincided with increasing levels of industry that continued into the 20th century.

4.1.3. Redditch was later designated as a New Town in 1964 which instigated a period of major development that saw the population grow from 30,000 to 80,000 (approximate) over 20 years. The majority of this residential development was experienced on the eastern side of the town beyond the River Arrow, but the town also spread west towards the site as the neighbourhoods of Batchley and Webheath were formed.

4.1.4. The towns transport network was overhauled throughout the process of New Town development and many of the historic routes were replaced or upgraded by the introduction of dual carriageways and other major A-roads. This included the A448 dual carriageway that runs along the north eastern boundary of the site linking Redditch with Bromsgrove.

4.1.5. The site itself appears to have always existed as agricultural land and a number of historic field boundaries remain today, particularly within the southern half of the site. The residential neighbourhood of Webheath is located adjacent to the site on its eastern boundary and was built towards the end of the 20th century; its urban form comprising low density sub-urban development. A number of nearby farms can be located on maps dating back to pre 1874 and these include Holyoake's Farm to the north west, The Hill Farm to the north east and Lanehouse Farm to the west.



Historic Settlement Growth plan





## CHANGING LANDSCAPE

4.1.6. Landscape and settlement character is shaped over time and an appreciation of the stages and activities that have happened in farming practices, in settlement and transport as well as commerce can all leave markers in the landscape that contribute to the ‘local distinctiveness’ (the phrase first invented by Common Ground in 1983) of a place. For a new development to have a successful relationship, and achieve a positive interaction with its human context as well as its landscape setting a clear understanding of the history of the place can provide invaluable parameters and prompts.

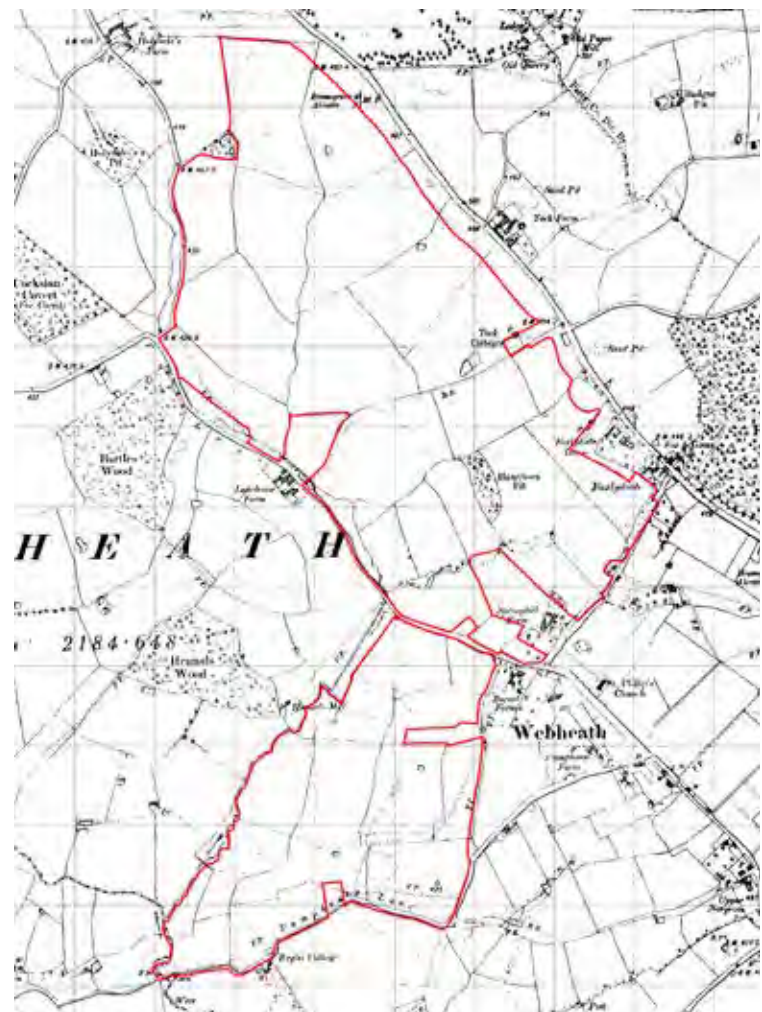
4.1.7. Change in the landscape has been recorded on Ordnance Survey maps for over 100 years and these maps give snap-shots of the site and its setting since 1905.



OS Explorer Map 220,204

### 1905

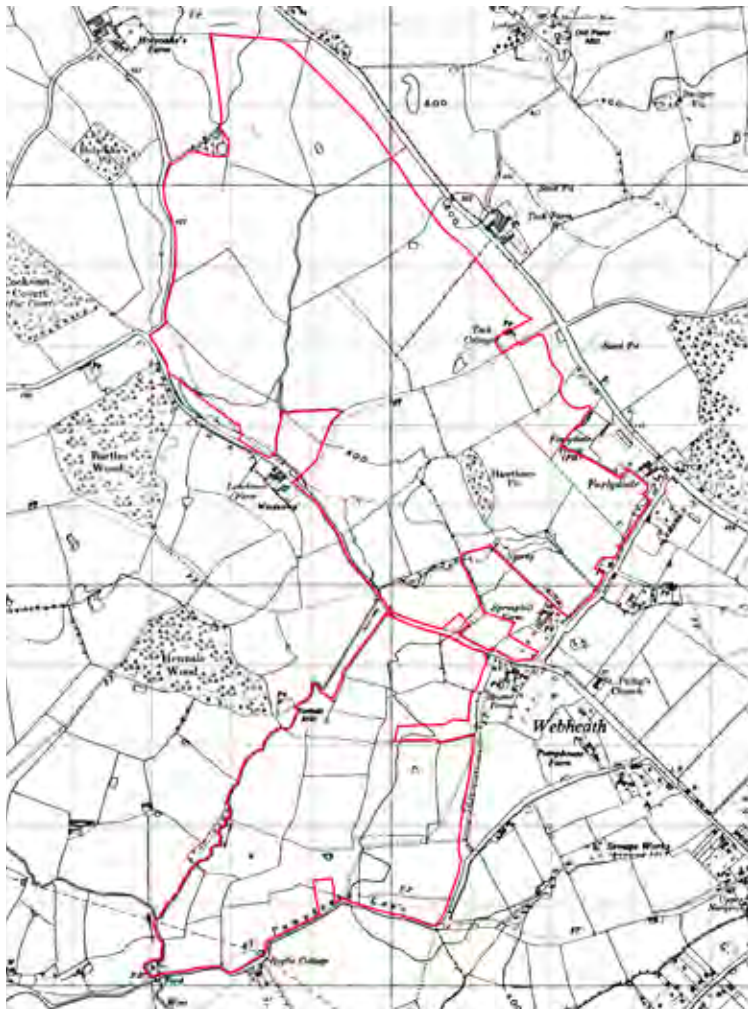
- » The fields of the site are recognisable but the northern area is more divided by hedgerow field boundaries
- » The A448 Bromsgrove Highway does not exist and Hewell Lane is the principle road
- » Webheath is a modest rural settlement comprising Springhill, Boxnot and Pumphouse Farms and St Philip’s Church
- » Hennals Wood is considerably larger than today



Historic Map 1905

*1955*

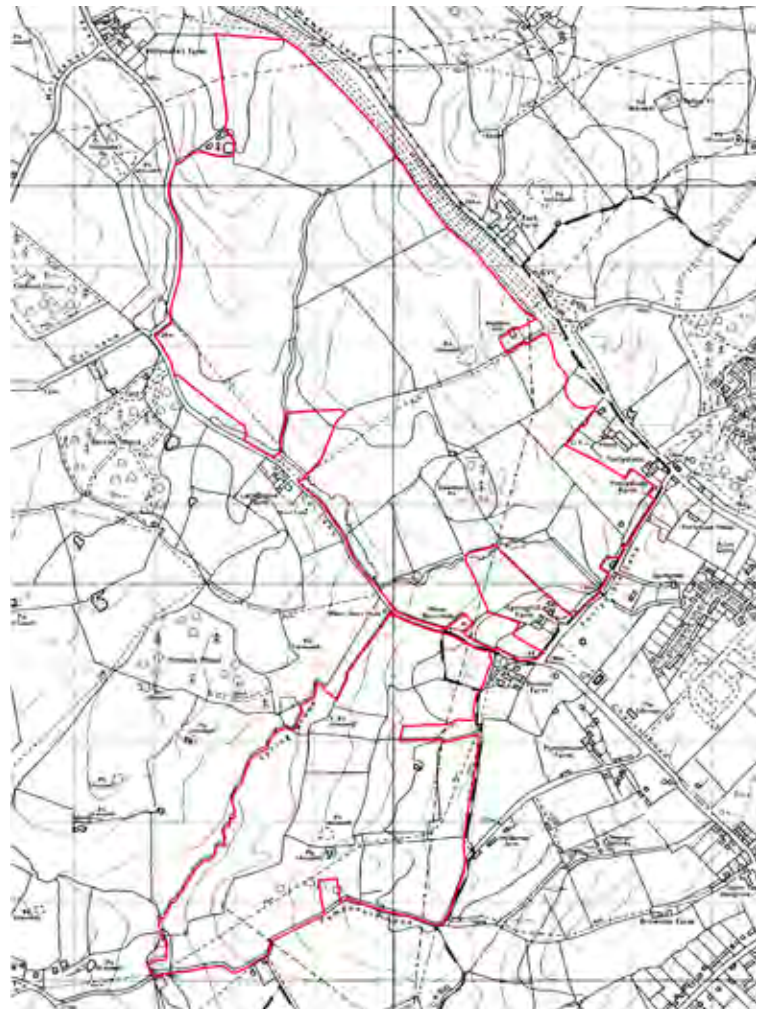
- » With change since 1905, the fields of the site are still recognisable but the northern area is still more divided
- » The A448 Bromsgrove Highway does not exist and Hewell Lane is the principal road
- » Webheath is still a modest rural settlement comprising Springhill, Boxnot and Pumphouse Farms and St Philip's Church
- » Hennals Wood is still considerably larger than today



*Historic Map 1955*

*1978*

- » The fields of the site are recognisable: the northern area is now more open with larger fields created
- » The A448 Bromsgrove Highway is partially built as far as the Foxlydiate junction
- » Late twentieth century development in Webheath has started and development to the north in Batchley has occurred
- » Hennals Wood is still considerably larger than today



*Historic Map 1978*



## 4.2. LOCAL CHARACTER

*The character and form of the proposed development should be influenced by the distinctiveness of its surroundings. Analysis of the existing built environment provides useful character references which will help to guide the creation of a new garden neighbourhood for Redditch.*

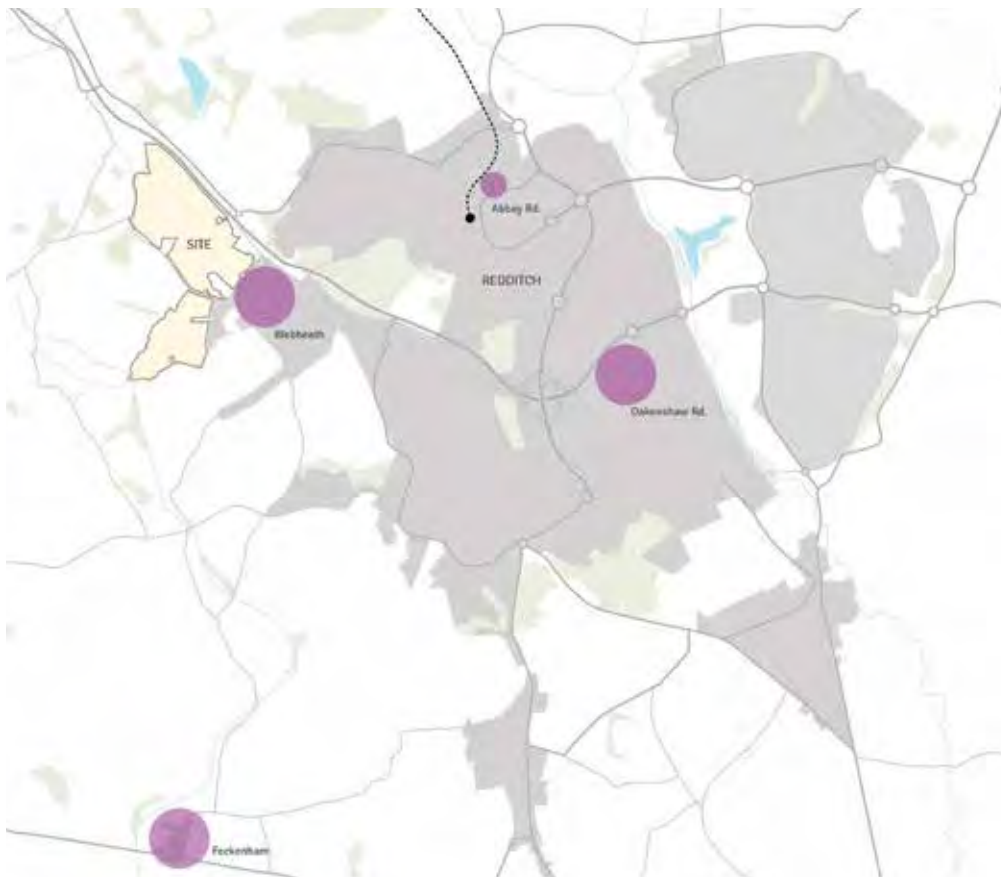
4.2.1. In order to achieve a design response that is distinctive and has a strong sense of place, five character areas have been chosen for the study, including examples within the borough of Redditch and district of Bromsgrove. The character areas are listed below:

- » Oakenshaw Road, Redditch
- » Abbey Road, Redditch
- » Webheath, Redditch
- » Feckenham: local village to the south of the site
- » Belbroughton: a rural village near Bromsgrove

4.2.2. The following design elements have been assessed in order to help identify the key characteristics of each study area:

- » Urban Form
- » Built /Plot Form
- » Car Parking
- » Open Space
- » Details and Materials

4.2.3. Together the study of these elements will help to inform the masterplan and illustrative material presented later in this document.



Character Areas Location plans



Opposite: Development located on the southern edge of Webheath







## 4.3. AREA 1: OAKENSHAW ROAD, REDDITCH

*Oakenshaw Road is a 1950s suburb to the south east of Redditch town centre. It comprises a suburban character.*

### URBAN FORM

- » The area is primarily laid with gently curving streets which create linear shaped blocks
- » Some blocks are larger, and incorporate small cul-de-sacs. This can impede pedestrian/cycle movement and legibility
- » Generally medium density development
- » Hierarchy of routes defined by street width and the use of street trees.

### BUILT / PLOT FORM

- » Largely semi-detached and terraced properties set within mid to large size plots with side access
- » Dwellings are set back within the plot to allow for a front garden and on plot parking
- » Dwellings are typically 2 storey.

### CAR PARKING

- » Car parking mostly provided on-plot
- » Mix of frontage and side access driveways, with some integral garages
- » Street widths allow for ad-hoc on-street parking between driveways.

### OPEN SPACE

- » Open space generally consists of SLOAP, usually found at junctions
- » Grassed verges are common features of this character area; they help to further emphasise a green and suburban character
- » Tree planting within verges help to enclose streets and form a valuable addition to the character of the area, particularly avenue street tree planting found on Oakenshaw Road.

### DETAILS AND MATERIALS

- » Dwellings generally comprise brick facades and clay roof tiles
- » Common use of bay windowed property frontages
- » Small brick walls and hedgerows are a consistent boundary feature of front gardens.



*Significant areas of tree planting aid the creation of a green, suburban character*



*General use of brown brick facades and simple and traditional building forms*



*Long, straight streets aid long distance views and help to establish a sense of rhythm*







## 4.4. AREA 2: ABBEY ROAD, REDDITCH

*Abbey Road is an example of a 1930's semi-detached street in central Redditch. It has a more formal character, defined by the common use of semi-detached properties and uniform materials and architectural details.*

### URBAN FORM

- » Abbey Road is a largely straight street, aiding the creation of a more formal character and allowing for long distance views to the west which are terminated with large scale tree planting. To the west, the street gently curves as it reaches Melen Street, providing a series of glimpsed views to 3 storey terraced properties.
- » Abbey Road runs along a contour therefore, there is a slight level change across the street. Long front gardens (4-6 metres) are used as appropriate, to deal with this change in levels.

- » The structure and form of development blocks are clearly defined; all dwellings front on to the street, aiding natural surveillance and activity.

### BUILT/PLOT FORM

- » Medium density development largely consisting of semi-detached properties set within large plots with side access. There are also some use of terraces
- » Dwellings along Abbey Road are 2 storey with 3 storey terraces where Abbey Road meets Melen Street. Levels changes across the street also create some variation in building height; this is effective in creating interest along the streetscene and emphasising focal buildings.
- » The use of a largely consistent building line emphasises a more formal character.

### CAR PARKING

- » Parking is provided on plot within original driveways located to the side of the dwellings.
- » Later alternations have seen some garages

introduced as side extensions with driveways expanded into front gardens.

- » Informal on street parking also occurs between driveway accesses.

### OPEN SPACE

- » Narrow grassed verge located on both side of the street which is broken by driveway access, with tree planting on both sides of the street within the verge.
- » Clipped hedges form boundary features in places

### DETAILS AND MATERIALS

- » Dwellings have mix of red brick and white render facades which provide an interesting contrast which contributes to the character of the street.
- » All properties have bay window frontages.
- » Chimneys and ornamentation around windows create interest and rhythm.
- » Properties have hipped roofs with clay plain-tiles



*Terraced properties and increased storey heights reflect a more formal character*



*The use of projecting architectural details adds interest to the street scene*



*Street tree planting and verges softens the appearance of the built form*







## 4.5. AREA 3: WEBHEATH, REDDITCH

*Webheath is an example of late 20th century suburban development which is located on the edge of Redditch adjacent to the site*

### URBAN FORM

- » Low density suburban development built in the 'root and branch' style comprising cul-de-sacs served by local distributor roads.
- » The layout is car orientated and offers poor connectivity and legibility for pedestrians and cyclists.
- » Building orientation is confused where properties side or back on to the main movement routes.
- » No definitive structure to the urban form, although the original field boundaries remain evident in some places.

- » Varied treatment of properties located at the edges of the urban area, some overlook the surrounding countryside with plot access provided directly or grouped by private drives.

### BUILT / PLOT FORM

- » Largely detached properties.
- » Buildings typically sit within mid to large size plots front gardens and narrow side access.
- » Predominantly 2 storey properties with some bungalows.

### CAR PARKING

- » Car parking is located on plot through the provision of frontage driveways
- » Most properties have integral garages
- » Opportunity to park informally on street in gaps between driveways.

### OPEN SPACE

- » Pockets of grassed open space provided but this generally isn't well overlooked by surrounding development, which could cause security issues.
- » Some of the original field boundaries are retained to provide established landscape corridors crossing the site. However the quality of these features is often degraded where they form private boundaries
- » Grassed verges and trees are used along primary movement routes, helping to soften the visual appearance of the built form.

### DETAILS AND MATERIALS

- » Dwellings generally comprise brick facades with some use of white render.
- » Properties typically have shallow pitched roofs which are finished with grey, red or brown concrete pantiles.
- » Residential boundary features usually comprise low level brick walls and / or clipped hedge at the frontage of properties.



*Facade materials largely comprise render and brick*



*Parking is predominantly on-plot*



*The use of cul-de-sacs can impede pedestrian movement and create low quality alleyways that are not overlooked by dwellings*







## 4.6. AREA 4: FECKENHAM

*Feckenham is a traditional Worcestershire village which is characterised by low density development and a variety of architectural styles.*

### URBAN FORM

- » Low density development
- » Development blocks are structured by primary movement routes
- » Informal development structure aids the creation of a rural character
- » Public space and landscape also shapes the urban form.

### BUILT /PLOT FORM

- » Generally large plots with a mix of detached properties and short terraced runs
- » Detached properties often sit within square plots, whereas terraced property plots are longer and more narrow
- » Dwellings tend to front directly onto the street without front gardens or boundary treatments
- » Properties are predominantly 2 storey.

### CAR PARKING

- » Mix of parking provision including on plot and on street
- » Some examples of grouped parking in rear courtyards, usually serving terraced properties.

### OPEN SPACE

- » Village green forms important element of the local character
- » Areas for formal sports provision form an interface between development and the surrounding countryside
- » Minimal street-trees and planting. However, trees within private gardens are common features which contribute to the green and rural character of the village.

### DETAILS AND MATERIALS

- » General use of red brick facades with occasional use of white render
- » Frequent use of clay plain-tiled roofs with a variety of roof pitch
- » Dormer windows are common, with some gabled facades.



*A variety of architectural styles and materials create a distinct and informal character*



*The use of red brick is a predominant character feature*



*Dwellings overlook the street with small front gardens*







## 4.7. AREA 5: BELBROUGHTON

*Belbroughton is an example of how development and public open space can positively respond to the existing topography to create an informal and village character.*

### URBAN FORM

- » Relatively low density development
- » Early development built along main streets with later additions to the rear creating large development blocks
- » Organic, ad-hoc development structure
- » A mix of dwellings fronting directly on to the street or set back with front gardens to create an interesting and varied street scene.

### BUILT /PLOT FORM

- » Generally large plots and dwelling sizes
- » Mix of detached and semi detached properties, with some short terraced runs
- » Detached properties often sit within square plots, whereas terraced property plots are longer and more narrow
- » A number of plots side onto the road to work with the sloping topography, creating a distinct character
- » Properties are predominantly 2 or 2.5 storey.

### CAR PARKING

- » Mainly on plot parking, with some parking on street and a small number of parking courtyards.

### OPEN SPACE

- » Green and informal areas of public open space are a distinctive character element of the village.
- » Open space tends to complement and respond to the sloping topography of the village
- » Informal tree planting, hedgerows and vegetation are all common features of the streetscene.

### DETAILS AND MATERIALS

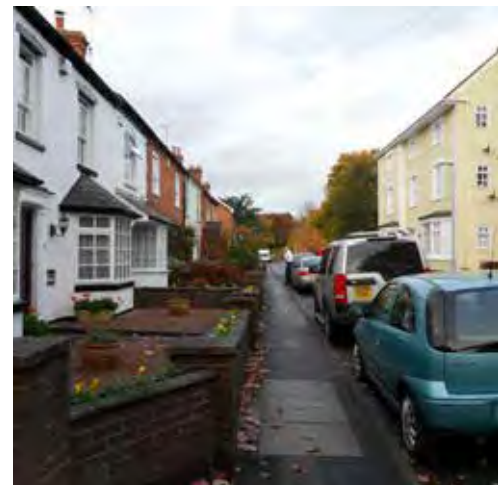
- » Predominantly red brick facades with the occasional use of render.
- » Chimneys and ornamentation around windows create interest and rhythm
- » Relatively shallow roof pitches with frequent use of clay plain-tiles
- » Occasional use of dormer windows adding variety to the roofscape.



*General use of red brick facades with focal buildings highlighted by the use of render*



*Informal pockets of open space respond to the topography*



*Terraced properties and increased storey heights are used along the High Street*









## 4.8. INSPIRATIONAL QUALITIES

*An analysis of each character area has been undertaken to help inform the design decisions within the masterplan and later detailed proposals for Monarchs Green. A number of character generators have been formulated as a result of this analysis, which are set out below:*





## URBAN AND BUILT FORM

- » Development should generally consist of 2 to 2.5 storey dwellings. Increased storey heights could be located along key routes and in other prominent locations to provide variation and aid a legible townscape.
- » Main streets and spaces should be overlooked by continuous building frontages where possible, with buildings set back in places to create interest along the street scene and provide a transition between the private and public domain.
- » The development should incorporate a range of densities that are responsive to uses, street types, topography and location within the site. This will aid the creation of a varied and distinctive townscape.
- » The development should ensure that plot sizes are appropriate to the size of the dwelling, which can be dictated by location, density and character.



## PUBLIC OPEN SPACE AND PLANTING

- » Street trees and provision of grass verges in the street, such as those found in Oakenshaw Road, Abbey Road and Webheath is successful in creating an attractive green environment which also helps to define street hierarchy and aid legibility.
- » Planting within front gardens should be used to soften the appearance of the built environment and can encourage locally distinctive street scenes. The scale of planting in front gardens should be considered to ensure that dwellings are not completely screened from view along the street, which would impact upon levels of natural surveillance.
- » The development should provide high quality open spaces that allow for relaxation, recreation and play in accessible and safe locations. Retention of existing established landscape features incorporated within open space helps to create an attractive environment.
- » Development layout and open space should respond positively to the topography and landform of the site, aiding a distinctive character.



## STREETS AND CAR PARKING

- » The development should incorporate a well connected and permeable network of streets and spaces that have a clear hierarchy which aids legibility and way-finding.
- » The alignment of streets contributes to defining the character of an area. Longer and straighter streets provide a level of formality and allow for long distance views, whereas shorter, more curved streets are less formal and create glimpsed views.
- » Car parking should generally be provided on plot, with additional spaces positively designed into the street scene, as appropriate. Some types of development, such as terraced dwellings, could have rear parking however, this should be infrequent and provided only in small groups.
- » Private drives are successful in allowing dwellings to attractively overlook green space.
- » The total level of car parking provided at the development should respond to the level of demand to ensure that parking does not take place in inappropriate locations that detract from the character of the place.



## ARCHITECTURAL STYLE AND DETAILS

- » The use of simple and traditional building forms will successfully emphasise the local vernacular of scale and enclosure.
- » A simple architectural style should be developed which is based on a local palette of materials, such as red/ brown brick, white render and plain roof tiles, with a restricted number of details.
- » Elements including single and double storey bay windows, dormer windows, pitches and hipped roofs, chimneys and detailing around windows and doors add interest to the streetscene and aid a varied townscape.
- » Residential boundary treatments should comprise low brick walls and planting where appropriate. Consideration should be given to the scale of planting.







*Chapter 5:*

# ACCESSING THE CONTEXT: THE SITE

## 5.1. ACCESS

*A Transport Assessment has been prepared for the site, in accordance with The National Planning Policy Framework (2012) and Planning Practice Guidance (2014). It addresses public transport capacity, walking and cycling capacity and road network capacity. It also considers the safety implications of the development, and develops transport strategies to overcome issues of congestion and severance.*

### PEDESTRIAN AND CYCLE ROUTES

5.1.1. A comprehensive assessment has been carried out of local routes for pedestrians and cyclists.

#### *Walking*

5.1.2. It is found that walk routes in the area are good in that: local roads are overlooked by residential properties; many routes are quiet with adequate footway widths and lit when dark; and, the residential areas permeable with convenient alley ways proving direct routes.

5.1.3. During the busiest hours of the day Birchfield Road can be more difficult to cross and there are no formal facilities in the vicinity. The A448 is a potential major obstacle, but there are at least three walk routes across it, including the Birchfield Road bridge, a wide subway route to Batchley and via Bromsgrove Road.

5.1.4. There are many leisure routes along Public Rights of Way and nearby country lanes, but these are sometimes spoiled by people driving too quickly along them.

#### *Cycling*

5.1.5. There is a comprehensive network of quiet streets on the south side of Redditch, suitable for any cyclists. Confident cyclists are provided with on-road advisory routes on Bromsgrove Road and Birchfield Road, which provide access to Headless Cross and the Town Centre. Again, local roads are ideal for cycling as they are overlooked by residential properties; they are quiet routes and lit when dark; and, permeable with convenient alley ways.

5.1.6. The National Cycle Network (Route 5 – Birmingham, Bromsgrove, Redditch, Stratford, etc) arrives in Redditch on Pumphouse Lane and serves Webheath on Church Road and Tynsall Avenue, before continuing to the Town Centre on Bromsgrove Road. The infrastructure is mostly wayfinding and on-street advisory cycle lanes in the urban area, which provides some protection for cyclists.



## BUS AND RAIL ROUTES

5.1.7. The area is well served by the local bus network, and there are opportunities to divert existing services into the site. However local residents have expressed concern about the quality and reliability of the services, and therefore their sustained viability. This could be mitigated with direct investment and revenues that come from this development and that on the former Webheath ADR site.

5.1.8. The local plan policy calls for all properties on the development to be located within 250m of a bus route, which is a far higher standard than is achieved in many areas in Redditch. However it may be possible to achieve this if the network around Webheath can be modified, and a more suitable route can be found.

5.1.9. Bus routes adequately serve Bromsgrove, Redditch Town Centre, other areas of Redditch and Alexandra Hospital. This is a good level of service to places of work, worship, healthcare, education, shopping and leisure. These routes also connect with Redditch Railway Station, which provides three trains an hour to Longbridge, Selly Oak, University and Birmingham New Street.

## VEHICULAR ROUTES

5.1.10. The local road network in Webheath is not congested. Heathfield Road presents a difficult obstacle for general traffic and public transport, owing to on-street parking issues, and traffic therefore diverts along Foxlydiate Lane and Blackstitch Lane. Parking around the schools in Webheath also creates localised issues for residents in that vicinity.

5.1.11. Birchfield Road, Bromsgrove Road and Church Lane are free flowing throughout the day, with minor delays observed in the peak hours on Birchfield Road at A448 Bromsgrove Highway, Tynsall Avenue, and Bromsgrove Road. The A448 Bromsgrove Highway provide free movement to all areas of Redditch, and is likely to form the primary route to access the development.

5.1.12. Congestion is mostly experienced in Bromsgrove at the A448 junction with the A38, and on the A38 towards the M5 and M42. It is understood that this is subject to a major scheme funding bid, being made by Worcestershire County Council to the Department for Transport, that will alleviate congestion and accommodate forecast traffic growth.

5.1.13. The Transport Assessment considers the operation of the local road network in detail, and is informed by Worcestershire County Councils Traffic Model (Bromsgrove and Redditch Highway Assignment Model). The scope of this assessment includes all major routes within the two districts, and provides the local authority with a wide range of information to assess the impact of the development. Using this information, alongside surveys carried out on a school day in June 2015, the Transport Assessment considers the operation of local roads in Webheath in detail. The results of this analysis are considered in full in the TA and summarised in Section 7.2.



*Foxlydiate Lane/Church Road/Cur Lane Junction*



*Cur Lane*



*Redditch Rail Station*

## 5.2. FLOODING AND DRAINAGE

### FLOOD RISK

5.2.1. In accordance with the NPPF, a FRA has been produced for this site. According to mapping produced by the Environment Agency (EA), the site is largely within Flood Zone 1 ('low' risk of fluvial and tidal flooding) with some areas in the west adjacent to the Spring Brook located within Flood Zones 2 and 3 ('medium' and 'high' risk respectively). The extent of the floodplain has been confirmed using hydraulic modeling. All built development will be located within Flood Zone 1, and is therefore compliant with the NPPF in this regard. Some surface water flow paths have been identified by EA mapping, associated with minor topographical depressions

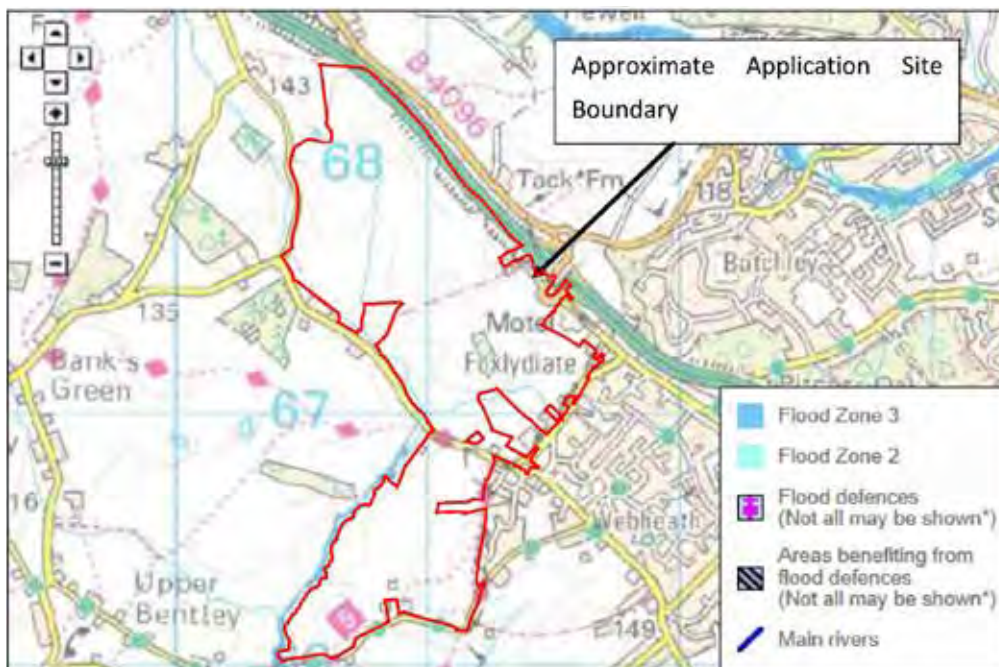
and ditches within the site. These flow paths will be accommodated within the proposals to ensure flood risk is not increased elsewhere. The site is partially located within EA designated groundwater SPZ's 1, 2 and 3. The EA have confirmed that the development is acceptable, subject to a number of criteria which are detailed within the supporting Drainage Strategy and FRA.

5.2.2. The site is not considered to be at risk of flooding from any other source. The development is considered to be appropriately located in this location, and will be appropriate in terms of flood risk for the development lifetime.

### DRAINAGE

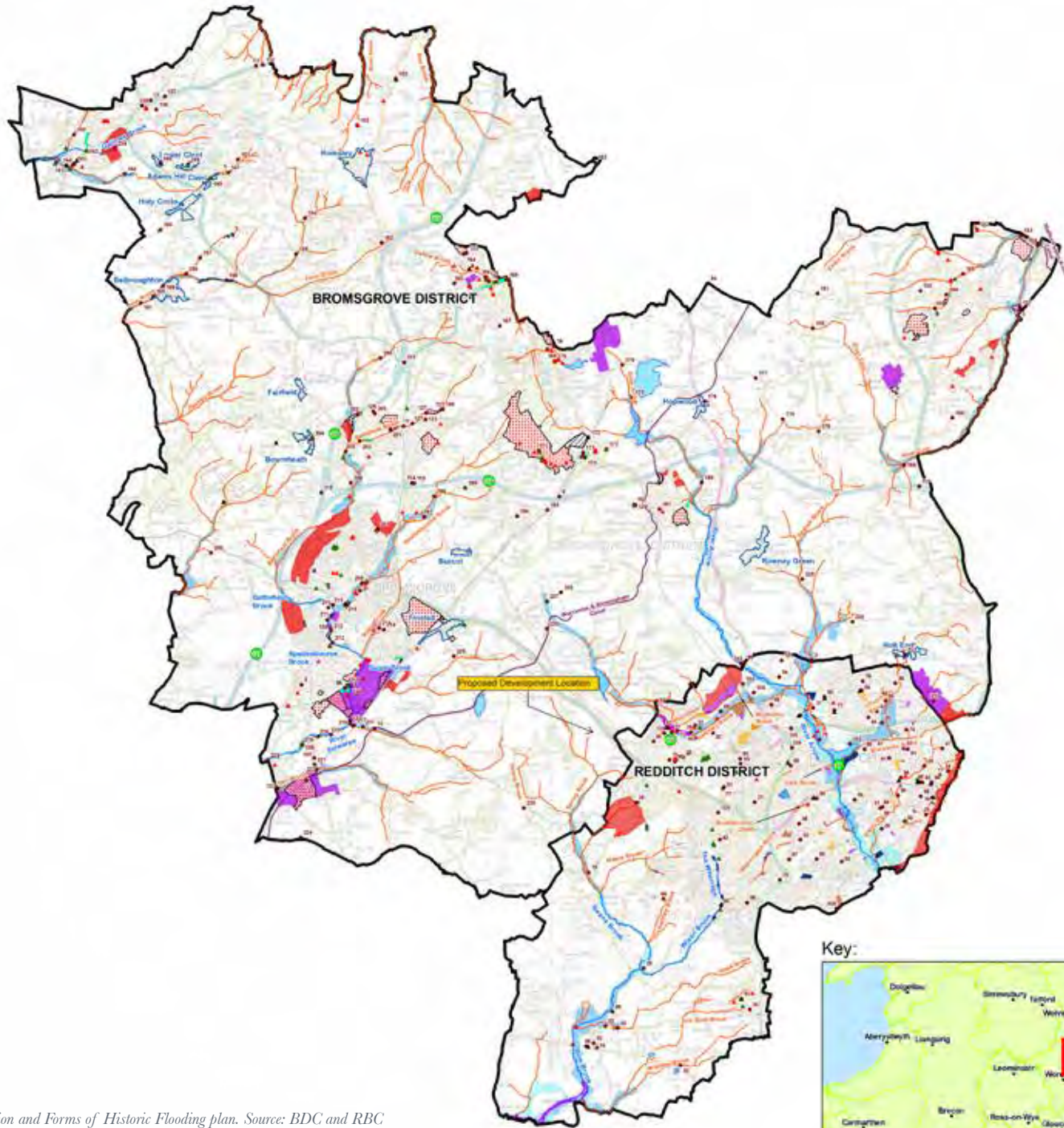
5.2.3. An outline foul and surface water drainage strategy has been produced for this site. The considerations of key stakeholders, such as the EA, Severn Trent Water (STW), and North Worcestershire Water Management has been incorporated throughout.

5.2.4. STW have undertaken modeling to assess the potential impacts of development proposals to the northwest of Redditch on the existing STW sewerage network. STW advised that upgrades to both the network and receiving treatment works will be required to accommodate the additional flows from the identified development sites. The drainage strategy will provide calculations to determine the proposed flows from the residential development to the foul sewer.



Flood Map for Planning (Rivers and Sea). Source: EA





Location and Forms of Historic Flooding plan. Source: BDC and RBC



Risk of Flooding from surface water. Source: EA

- Sewer Flooding**
- Foul Sewer
  - Storm Sewer
  - Unspecified
  - Historic Flooding Point
  - Historic Flooding Over Broad Area
  - Reservoirs, Lakes and Pools
  - Balancing Ponds
  - Ordinary Watercourses
  - Bromsgrove and Redditch Boundaries
  - Historic Flood Outlines
  - Canals
  - Main Rivers
  - Flood Zone 3
  - Flood Zone 2
- Development Sites**
- Village Envelopes
  - Policy Reference
  - Strategic Sites
  - Housing Sites
  - Employment Sites
  - ADR
  - Shopping
  - Unzoned

## 5.3. LANDSCAPE SETTING

*The Landscape and Visual Assessment has recorded how the site has clear physical elements that are particularly well defined at its edges.*

5.3.1. These topographical features: landform and the prominent patterns of native planting will ensure the physical containment along the site boundaries. The site has clear, successful limits that will prevent sprawl.

5.3.2. The following points provide a summary of site assessment;

- » The higher ground of the site is still lower than the ground beyond the site's boundaries north of the A448 and south into Webheath.
- » The shape of the landform, are strong features that define clear and logical limits to the site and the future development.
- » The containment along the boundaries is also made by existing planting in the hedgerows of Cur Lane and Gypsy Lane containing numerous prominent trees.
- » The lanes form strong boundary lines that can effectively give form and character to the edge of the new development with sympathetic effectiveness.
- » The A448 along the site's north eastern edge has landscape belts of structure planting.
- » The dual carriageway also has significant earthworks that add to the definition of the edge of the site.
- » The Spring Brook along the future countryside boundary of the southern area also has associated tree and hedgerow lines that further strengthen its line.
- » Again it forms a strong 'near natural' boundary line that can effectively give form and character to the edge of the new development with sympathetic effectiveness.
- » The south eastern edge of the site sits along the current boundary of the settlement.
- » Webheath is on land that rises higher than the site along this boundary and again containment is formed by existing elements: the rising landform; the numerous mature trees; and the buildings of Webheath at the edge of the town of Redditch.



*View from Foxlydiate Lane towards the Hawthorn Pit*





*View towards the northern site boundary*



*View towards the north eastern site boundary*



*View towards existing development in Webheath in the southern area of the site*



## 5.4. LANDFORM

*Topography is the study and mapping of the features on the surface of land, including natural features such as valleys, scarps and lakes and water courses as well and constructed features such as roads and lanes.*

5.4.1. Landform is more specific to just the natural physical features of the land and its surface. In the landscape there is a close association between these terms.

5.4.2. The desk based study and analysis of the local setting landform is presented on the Site Landform plan. This is a record of the on-site fieldwork: the topographic site specific survey, ground levels, hedges, services etc.

5.4.3. At its broadest level landform influences the topography and forms a fundamental element in the setting for the site. It is heavily related to geology and soil as it creates, through natural process or the intervention of man, the form of the land of the site and the setting. It is also, therefore, a major determinant in the significance of the visual qualities of the site in terms of prominence or concealment. The man made interventions have included the construction of the A448, Bromsgrove Highway and at a small scale, the digging on the lanes: Cur Lane, Gypsy Lane and others in the local setting. Agricultural enclosures and the planted woods and copses have made a pattern of enclosure across the site that produces a pattern of enclosure with a range of scale: from the expansive scale of the high ground at the north eastern top of the site to the smaller scale of the more enclosed fields along the settlement edge.

5.4.4. The landform is distinctive: rising by approximately 61m from the low point of approximately 95m AOD at the south western 'tip' near the Spring Brook, to

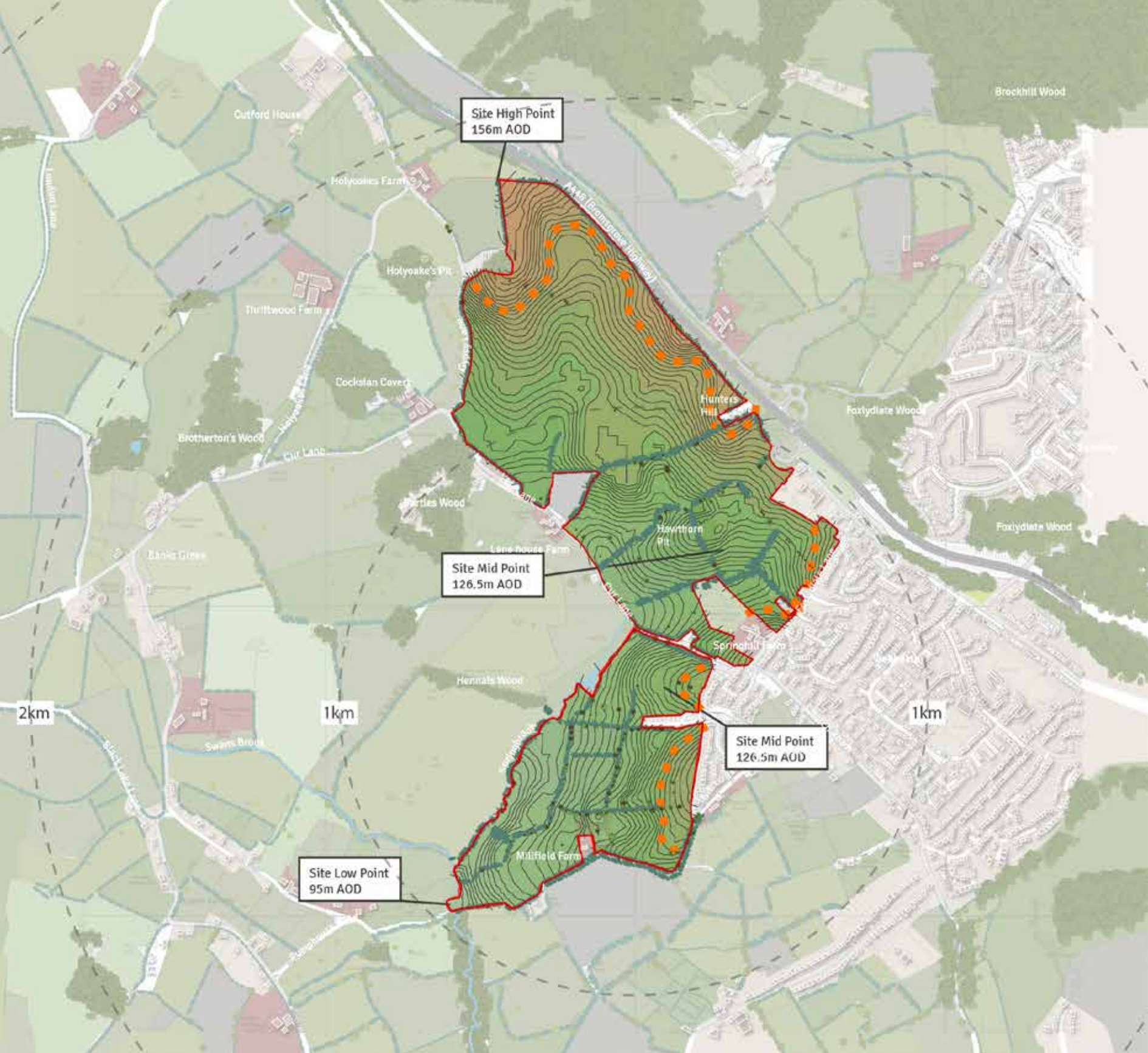
156m AOD on the northern boundary near Holyoake's Farm close to the A448. Within these limits there is considerable variation in slope and orientation of the landform. There is a pronounced west to south westerly aspect to the slopes and roll in the landform of much of the site. The long run of the land from the edge of Webheath between Cur Lane and the A448 has this generally westerly orientation. The most southern and eastern areas, where the site sits between the settlement edge and the Spring Brook, face a more north easterly direction.

5.4.5. Within these two broad areas defined loosely in terms of the orientation of the landform there is a considerable variety in elevation. The character of the landform has a rolling and undulating quality that with the pattern of enclosure produces variety and distinctiveness to the ground. The topography: the combination of landform and features on the land, create a clear and character defining attribute.

5.4.6. Consideration of landform is an important matter within the evaluation process of this LVA. Landform alone however cannot, and of course is not, the only consideration in the evaluation of the merits of a proposed change of use of the land, but it does have significant role to play in the consideration of the suitability, or not of the site, for development.

5.4.7. The site has comparatively gentle gradients: typically between 1:8 and 1:31 and these are appropriate for development use.





Site Landform Plan



## 5.5. ECOLOGY

*There are two Sites of Special Scientific Interest, two Local Nature Reserves, 13 Local Wildlife Sites and two areas of ancient woodland within 2km of the Site. The closest designation is Bow, Shell, Swan and Seeley Brooks LWS which is located directly adjacent to the south-western corner of the Site which has been designated due to its open flowing water with associated broadleaved, mixed and yew woodlands and reedbeds supporting species including otter and kingfisher.*

5.5.1. A Preliminary Ecology Appraisal was undertaken at the Site in March 2015 which included an Extended Phase 1 Habitat Survey. The habitats that were recorded on Site include arable fields, semi-improved and improved grassland, broadleaved woodland, species-rich and species-poor hedgerows, scrub, tall ruderals, buildings, hardstanding, standing open water and wet watercourses.

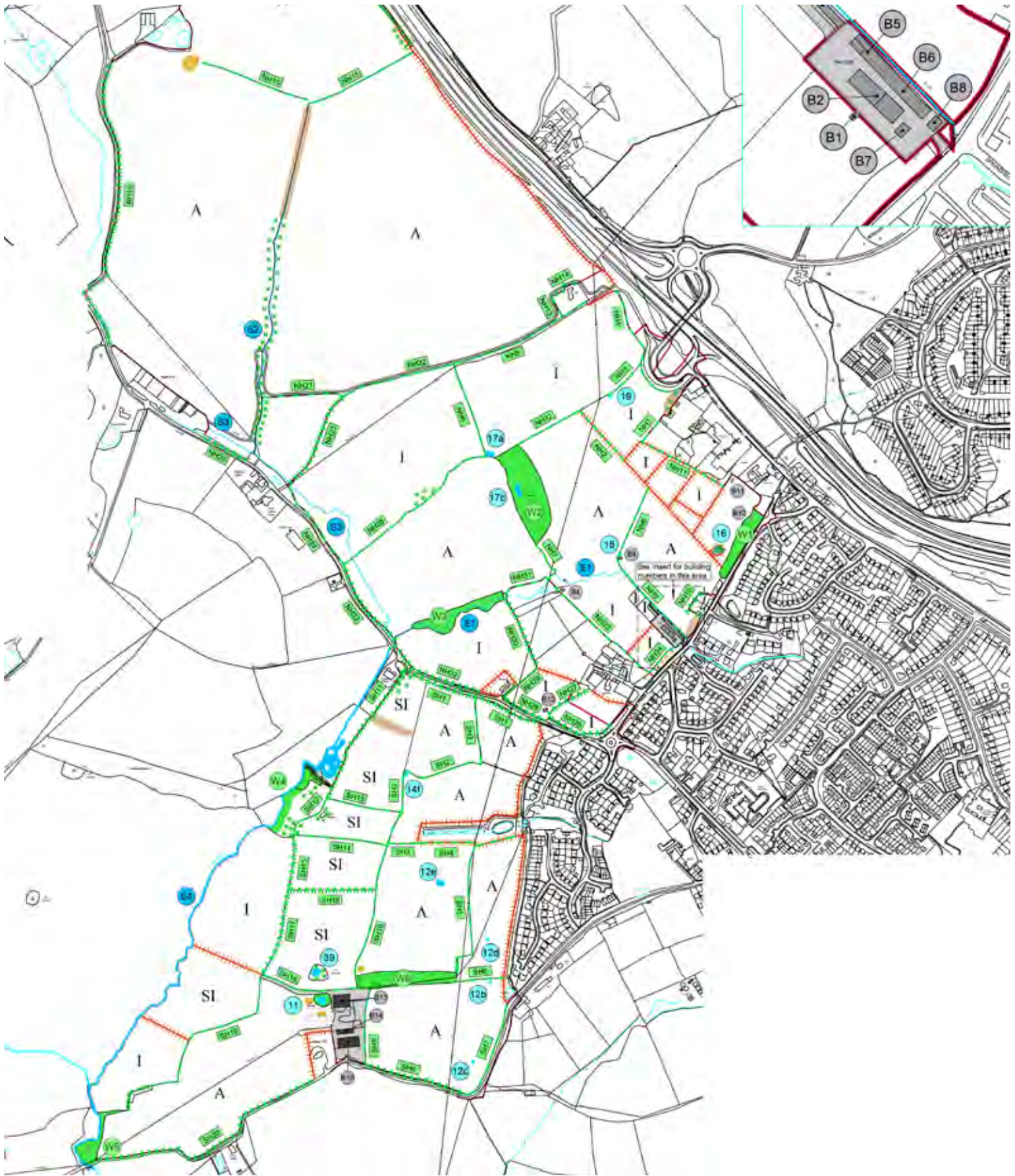
5.5.2. The results of the Preliminary Ecology Appraisal informed the need for a variety of ecology surveys, including amphibians, badger, bats, breeding birds, dormouse, hedgerows and invertebrates. These have been undertaken and found:

- » Badger – There is confirmed presence of badger on Site. It has therefore been recommended that the Masterplan design aims to retain wildlife corridors wherever possible.
- » Great Crested Newt – GCN within one pond within 500m of the Site were recorded during the update GCN surveys in 2015. Previous surveys in 2011 and 2014 identified GCN within four other ponds, one of which is located on Site. It has therefore been recommended that the Masterplan design aims to retain all standing open water wherever possible.
- » Bats – Bat activity and automated surveys were undertaken during the 2014 bat active season. Bat activity was concentrated along the hedgerows and woodland edges. Common pipistrelle were by far the most frequently recorded species within all areas of the site. Soprano pipistrelle, noctule and Myotis calls occurred in similar abundance but at

considerably lower levels. It has therefore been recommended that the Masterplan design aims to secure habitat connectivity across site by retaining as much of the existing hedgerow and tree cover as possible.

- » Bats – From the tree climb inspection surveys undertaken in 2015/16 no confirmed roosts were identified within the Site. However 21 trees scheduled for removal were considered to be category 2; low potential for supporting roosting bats and may require further surveys if they are to be removed.
- » Bats – One building within the Site, Building B3 on the Habitat Plan, is considered to be of moderate potential for supporting bats during the active season as a transitional or occasional day roost. It is not considered to be suitable for supporting hibernating bats or a maternity colony. Further surveys of the building have been recommended if the building is to be removed as part of the Proposed Development.
- » Birds – The results of the 2015 breeding bird survey indicated that the Site is of local significance for birds. It has therefore been recommended that the Masterplan design aims to retain hedgerows and mature trees wherever possible.
- » Hedgerows – The 2015 hedgerows survey established that 28 of the 54 hedgerows on Site are considered to qualify as ‘important’ under the wildlife and landscape criteria of the Hedgerow Regulations. All hedgerows provide suitable foraging and breeding habitat for





Habitat plan

<b>REFERENCE</b>				
Site area	-----	Disturbed ground	-----	Pond reference number
Arable field	A	Building	-----	Hedgerow reference number (North)
Improved grassland	I	Fence	-----	Hedgerow reference number (South)
Semi-improved grassland	SI	Intact species rich hedgerow	-----	
Broad-leaved woodland	-----	Intact species poor hedgerow	-----	
Muck heap/spill mounds	-----	Defunct species rich hedgerow	-----	
Scrub	-----	Defunct species poor hedgerow	-----	
Tall ruderals	-----	Building reference number	-----	
Waterbody	-----	Watercourse reference number	-----	
Flowing watercourse	-----	Woodland reference number	-----	

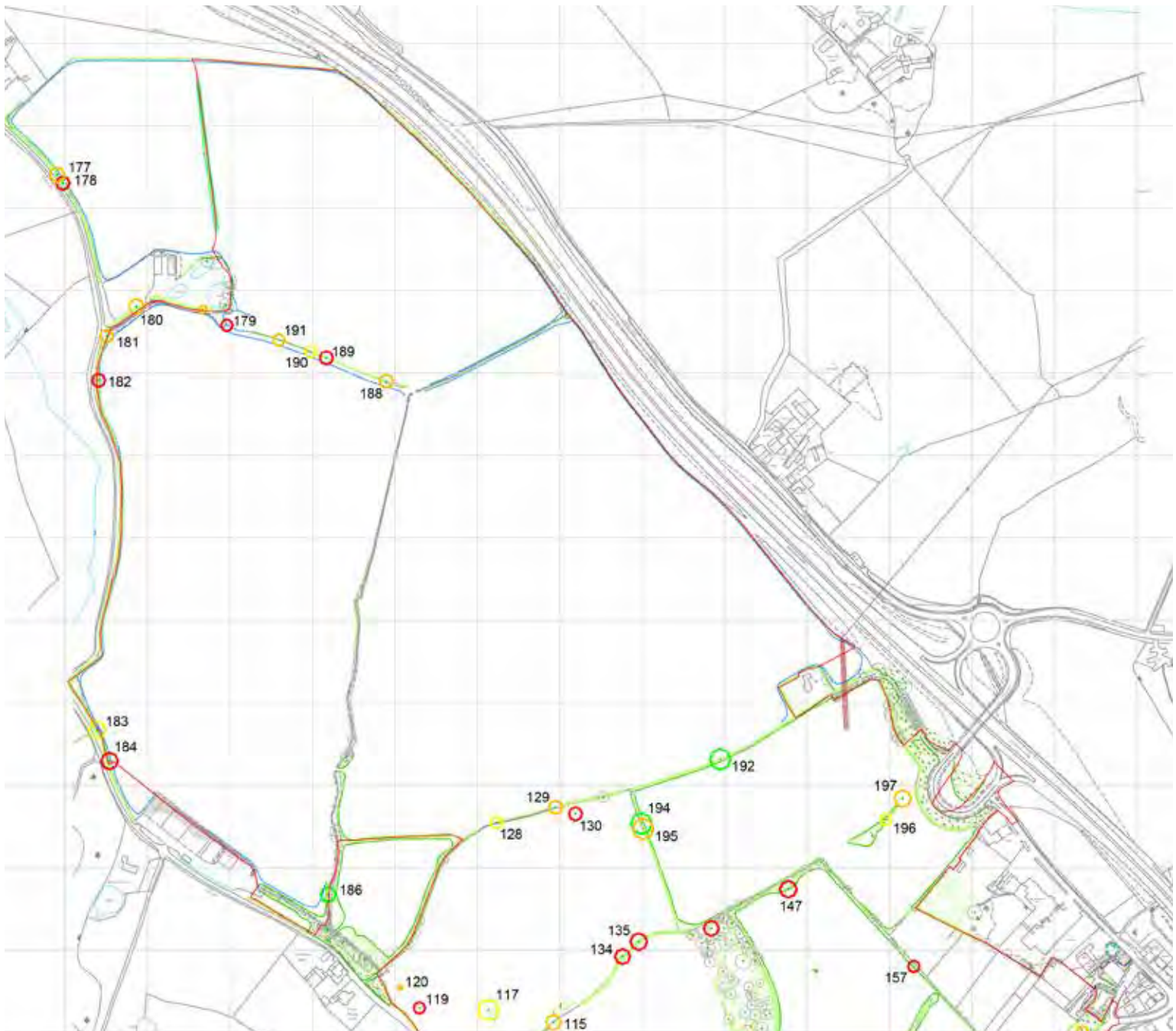


small mammals, invertebrates, birds and bats. It has therefore been recommended that the Masterplan design aims to retain hedgerows wherever possible.

- » Dormouse – No signs of dormouse were found during either the 2014 survey or the update nut search in 2015.

- » Invertebrates – A feature based assessment was undertaken in February 2016 and a sampling survey will be undertaken in spring 2016. Five trees were assessed as having moderate potential to support saproxylic invertebrates. These five trees will be subject to a sampling survey in spring 2016.

- » Otter and Water Vole – The 2014 survey concluded that the habitats on Site were largely unsuitable for both otter and water vole. There are opportunities to enhance the watercourses on Site for otter which are present within the wider landscape.

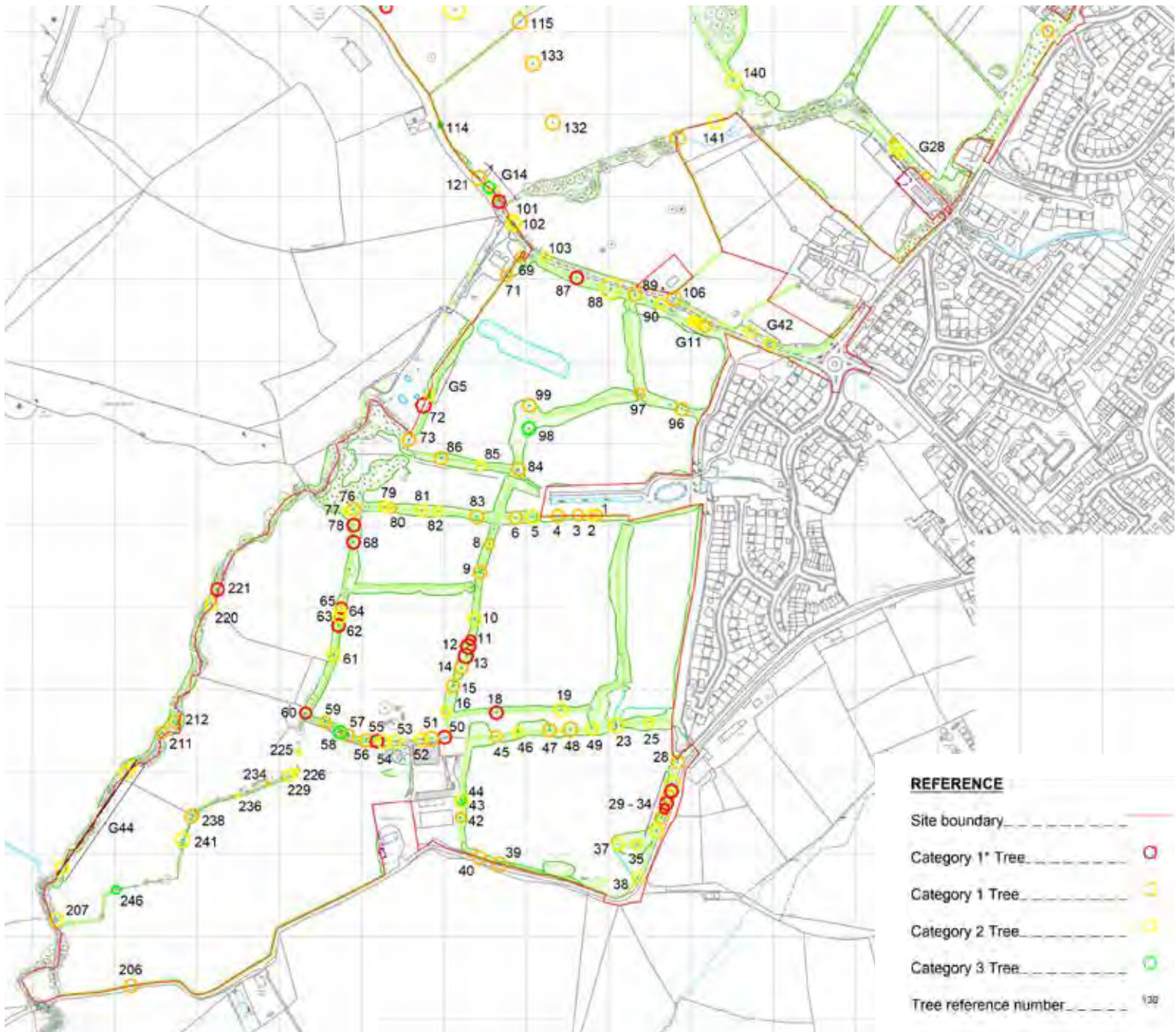


Trees with bat roost potential plan



5.5.3. Following the acquisition of baseline data, a detailed ecological impact assessment will be undertaken in accordance with the Chartered Institute of Ecology and Environmental Management's Guidelines for Ecological Impact Assessment in the UK

and Ireland: Terrestrial, Freshwater and Coastal (2nd Edition, 2016). The results of the assessment will be used to inform the mitigation measures and enhancements that will be incorporated in to the Masterplan design.



Trees with bat roost potential plan

## 5.6. HERITAGE

*The Environmental Statement (ES) has assessed the likely significant effects of the Proposed Development in terms of archaeology and cultural heritage in the context of the Site and surrounding area. In particular the likely effects of the Proposed Development on any known or potential heritage assets within the Site and the surrounding area during construction and operation have been assessed.*

Through a consultation with Bromsgrove District Council (BDC) and in line with current guidance and industry best practice a desk study of the baseline has been provided. The desk study has utilised the following sources:

- » Worcestershire Historic Environment Record (HER);
- » Worcestershire Record Office;
- » GIS datasets (Historic England 2015);
- » Scheduled Monuments;
- » Listed Buildings;
- » Registered Parks and Gardens;
- » Registered Battlefields; and
- » The National Heritage List for England (Historic England website).

5.6.1. In addition, a geo-physical survey has been undertaken to identify any archaeological remains and a confirming walkover survey of the Site has been undertaken.

5.6.2. The assessment has considered the effects of the constructional and operational phases of the Proposed Development on the following sensitive receptors:

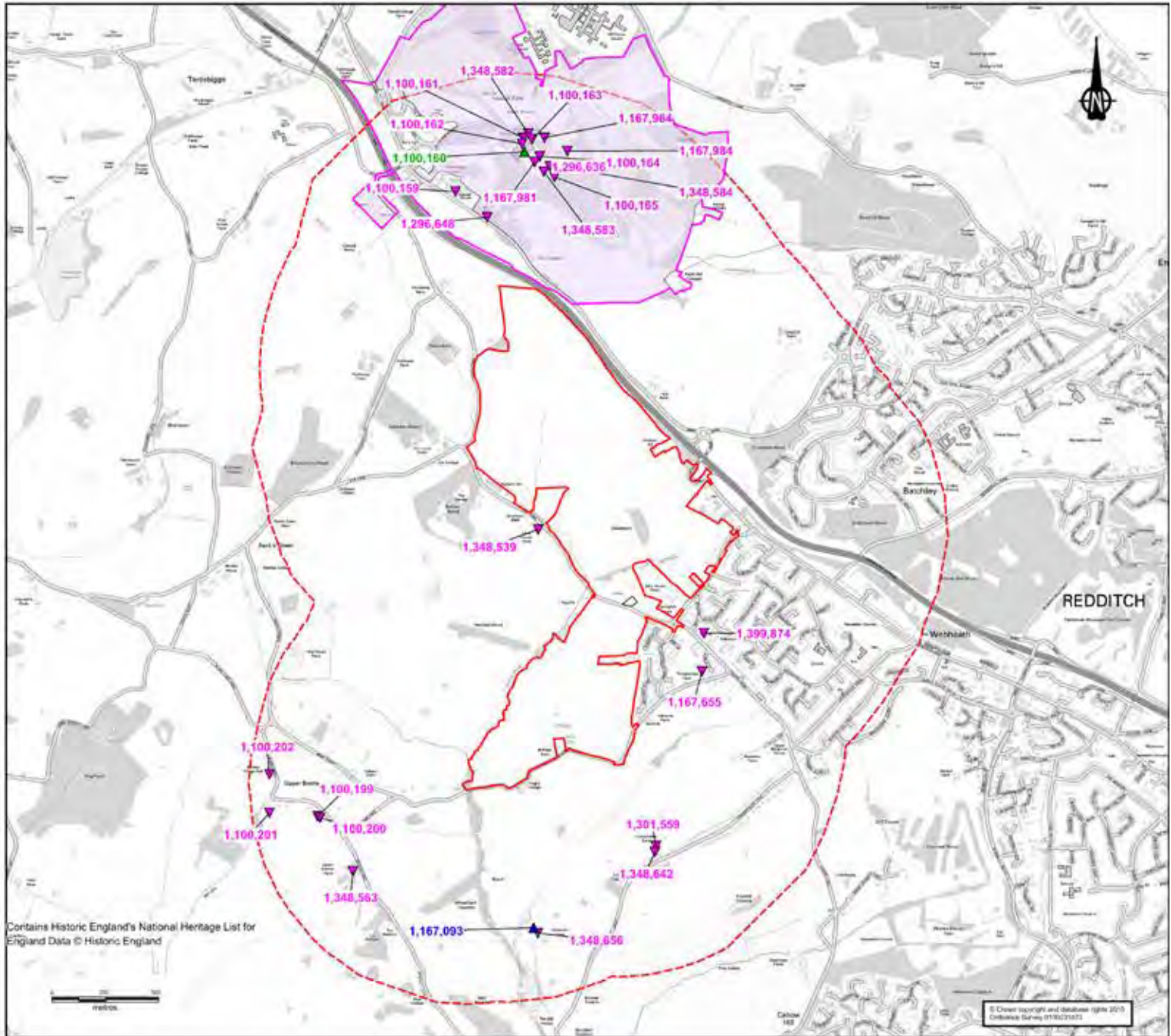
- » HER entries;
- » Previously unrecorded archaeology;
- » Hewell Grange CA and RPG;
- » Hewell House;
- » Lanehouse Farmhouse; and
- » Norgrove Court.

5.6.3. The assessment established that no designated heritage assets would be physically impacted upon by the Proposed Development.

5.6.4. A geophysical survey has not identified any areas comprising significant large-scale archaeological remains. Therefore, it has been concluded that no archaeological remains are present within the Site which could preclude development and that further work would not be necessary to determine the planning application.

5.6.5. In addition, the effects of the Proposed Development on the setting of Norgrove Court, Lanehouse Farmhouse and Hewell Grange Registered Park and Garden and Conservation Area would not be significant.





*Designated Heritage Assets plan*

-  Site Boundary
-  Search Area (1km)
-  Howell Grange Registered Park and Garden
-  Howell Grange Conservation Area
-  Grade I Listed Building
-  Grade II\* Listed Building
-  Grade II Listed Building

## 5.7. UTILITIES AND NOISE

### UTILITIES

5.7.1. A Utilities Statement has been produced. This has involved obtaining records to show the location of existing utilities and a Utilities Plan has been prepared to illustrate where the identified utilities are located within the red-line boundary and immediate vicinity of the proposed development. The Utilities Statement highlights the following are present:

- » An Esso oil pipeline is located in the northern part of the Site.
- » A National Grid local high pressure gas pipeline is present in the northern part of the Site.
- » Western Power electric transmission lines cross the Site, 11kV and 66kV overhead lines.
- » Zayo fibre optic telecommunications cross the Site, mainly following the high pressure gas pipeline route.
- » A Severn Trent Water mains are present, crossing in eastern and southern part of the Site as well as following perimeter along roads around the wider Site. Surface water highway drains are present along Cur Lane, Foxlydiat Lane and Birchfield Road, as well as the A448. Foul water drainage exists within the housing estates located to the east of the development and running along Foxlydiat Lane and Birchfield Road.

5.7.2. The Utilities Statement has recommended that oil and local high pressure gas pipelines are left in situ and are accommodated in the masterplan through applying easement distances. Diversions and reinforcements will be needed, however these are not uncommon to this type of development.

5.7.3. Each of the utilities have confirmed connection to their networks is achievable but will require more detailed investigation to provide detailed technical and financial appraisals for connections and network alterations once the Proposed Development is at the detailed design stage.

5.7.4. The investigation into utilities for the Proposed Development has determined that it is deliverable with respect to the ability to connect to the utilities networks.

### NOISE AND VIBRATION

5.7.5. The impact of noise and vibrations arising from the construction of operation of the Proposed Development has been assessed as part of the Environmental Statement (ES). The assessment has identified the existing sensitive receptors (i.e. existing residential properties) and the proposed sensitive receptors (i.e. proposed residential properties) that would likely be effected by noise and vibrations during the constructional and operational phases of the Proposed Development. The assessment has considered the effects of the following:

- » The impact of noise from the construction phase of the Proposed Development, including;
  - » The impact of noise from the construction of Site access roads on existing sensitive receptors, and proposed sensitive receptors which become occupied before and after completion of the Proposed Development.
- » The impact of vibration from the construction phase of the Proposed Development, including;
  - » The impact of vibration from the construction of Site access roads and development buildings on existing

sensitive receptors and proposed sensitive receptors which become occupied before completion of the Proposed Development

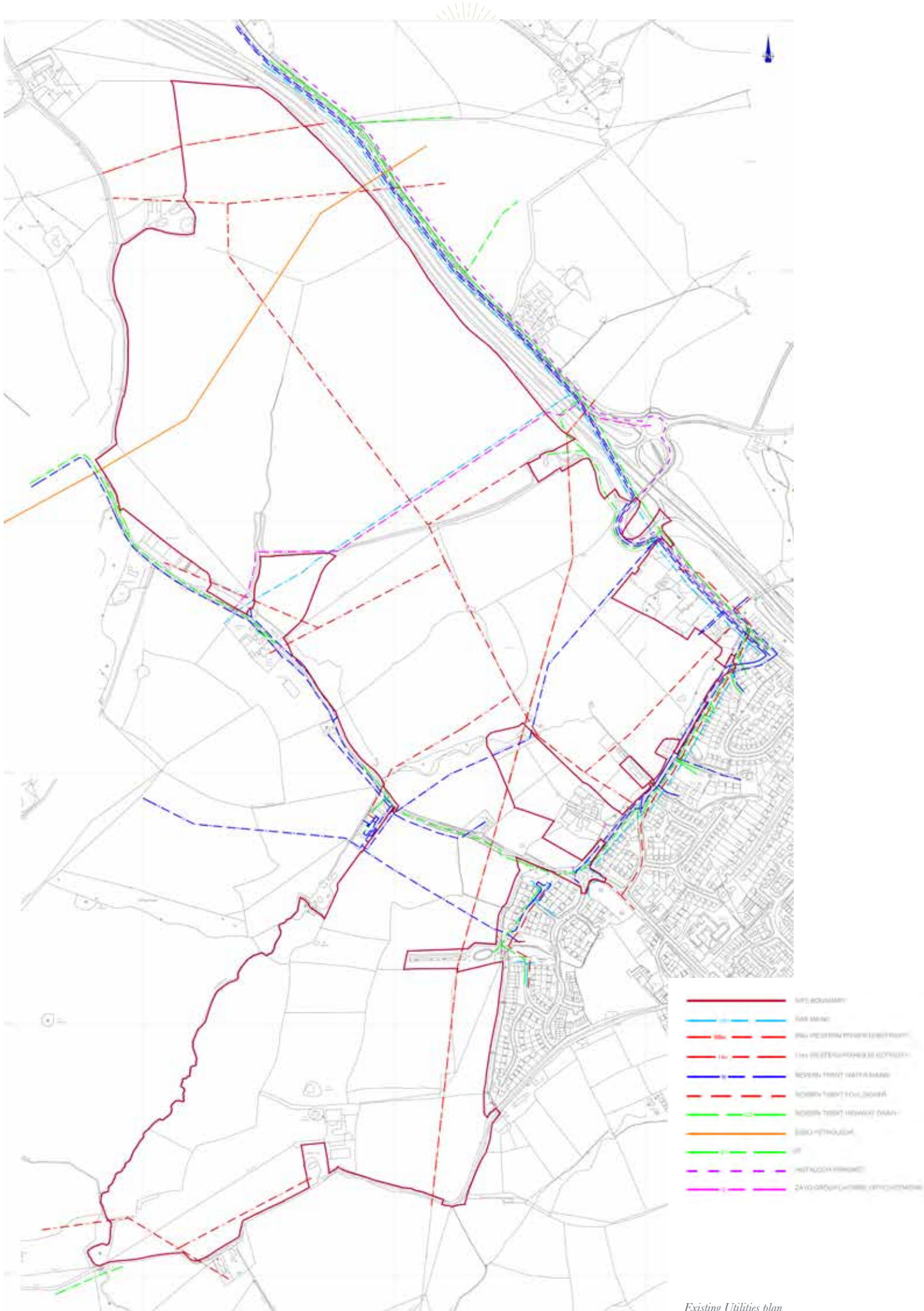
- » The impact of noise from the operational phase of the Proposed Development, including;
  - » Road traffic noise from vehicles associated with the Proposed Development on the existing and proposed road network on existing sensitive receptors;
  - » Noise from road traffic vehicles associated with the Proposed Development and existing traffic on the existing and proposed road network on proposed sensitive receptors; and
  - » Any existing noise associated with existing mixed use areas on proposed sensitive receptors and any noise from the proposed mixed use area on existing and proposed sensitive receptors.

5.7.6. With the implementation of mitigation measures (including restriction on working hours, the implementation of temporary screening, and best working practice) the effect of noise and vibration associated with enabling works and construction phases on existing and proposed sensitive receptors will not be significant.

5.7.7. In addition, with the implementation of mitigation measures (i.e. appropriate glazing and ventilation) the effect of current and future traffic on the noise levels at existing and proposed sensitive receptors will not be significant.

5.7.8. The assessment also concluded that mitigation measures may be required to ensure that the noise from the proposed mixed use area does not exceed the background noise level. These measures will be identified during the detailed design stage.





Existing Utilities plan

## 5.8. SUMMARY OF CONSTRAINTS AND OPPORTUNITIES

*A comprehensive analysis of the site has been undertaken and the findings evaluated to identify key opportunities and constraints relevant to the development of the site. This will ensure that the development at Monarch Green is responsive to important site features and can deliver the vision set out at the beginning of the document.*

5.8.1. The following points provide a summary of the site assessment and the opportunities it provides;

### LAND USE

- » Ensure the amenity and setting of existing properties that front or back on to the site is respected.
- » The development should explore the opportunity to provide new retail, community and education facilities, benefiting the new and existing community and ensuring that key facilities are accessible for new dwellings.
- » Be inspired by positive elements of the existing built form (as identified in section 4) to help aid a distinct character and sense of place.

### ACCESS AND MOVEMENT

























- » Vehicular access will be taken from Birchfield Road, Foxlydiat Lane, Cur Lane and Pumphouse Lane.
- » The development should promote the use of sustainable transport, ensuring that journeys by foot, bus and bike are an attractive option.
- » The development should retain and enhance connections to existing footpath and cycle linkages wherever possible, including existing Public Rights of Way, the Bridleway and Monarch's Way.

### DRAINAGE AND FLOODING

- » The majority of the site lies within Flood Zone 1 – low risk of fluvial flooding. There is a small flood plain associated with the Spring Brook; development will not be located within this area.





-  Site Boundary
-  Primary Vehicular Access
-  Secondary Vehicular Access
-  A448 and associated junction
-  Public Rights of Way (with associated pedestrian access to site)
-  Bridleway (with associated pedestrian access to site)
-  Monarch's Way
-  Utilities Routes and Associated Easements
-  HSE Consultation Zone
-  Boreholes
-  Noise offset to A448
-  Existing Ditch/Watercourse
-  Existing Built Development
-  5m contours
-  Trees
-  Hedgerow/Shrub Planting
-  Lane House Farm - Grade 2 Listed
-  Landform Orientation
-  Hewell Grange Conservation Area
-  PROW Viewpoint
-  Local Viewpoint
-  Private Land Viewpoint
-  Boundary contained by settlement
-  Boundary contained by tree cover



## LANDSCAPE AND TOPOGRAPHY

- » Use existing important landscape features and views to form a strong and connected green infrastructure that shapes the development.
- » Understand and respond to the landform and topography of the site.
- » Existing green infrastructure that includes copses, hedgerows and tree planting will be retained wherever possible and used to provide a strong landscape framework.

## ECOLOGY

- » Ecology assessments and protected species surveys have been undertaken to ascertain the current ecological value of the site. This data will be used to protect and enhance habitats of ecological value wherever possible, and to create a net gain in biodiversity.
- » The development will seek to maximise opportunities to enhance biodiversity and ecology wherever possible. This will be implemented through the creation of green corridors that are integrated with existing tree and hedgerow planting and the Spring Brook.

## HERITAGE AND ARCHAEOLOGY

- » A detailed study of the setting and visibility of Hewell Grange Conservation Area and Norgrove Court will inform the proposals.
- » The setting of Lane House Farm (Grade II Listed) will be respected.

## UTILITIES

- » Surveys have been undertaken to identify existing utilities crossing the site. Routes will be incorporated into the masterplan by making provision for appropriate offset distances or diversion.
- » The location of residential development and proposed densities will accord with the HSE consultation zones associated with the high pressure gas pipeline that crosses the site.

## NOISE

- » An appropriate noise offset and/or mitigation will be provided where the site boundary adjoins the A448.



*Existing dwellings adjoining the south eastern site boundary*



*Existing tree and hedgerow planting in the southern area of the site*



*Existing overhead powerlines cross through the site*





*Pumphouse Lane*







*Chapter 6:*  
**EVALUATION**

# 6.1. SUMMARY OF COMMUNITY AND STAKEHOLDER INVOLVEMENT

*The applicants recognise that community involvement is at the forefront of the planning agenda and is reflected in the principles underpinning the Planning and Compulsory Purchase Act (2004) and Localism Act (2011).*

6.1.1. This is further reinforced by the direction provided by the National Planning Policy Framework (NPPF), published in March 2012, which specifically encourages pre-application engagement with key stakeholders and the local community.

6.1.2. The Hybrid Planning Application proposals are the product of an extensive period of preparation, which has included consultation with the local community, Bromsgrove District Council (BDC) Redditch Borough Council (RBC) and Worcestershire County Council (WCC), along with key statutory stakeholders.

6.1.3. The consultation strategy included the following key events;

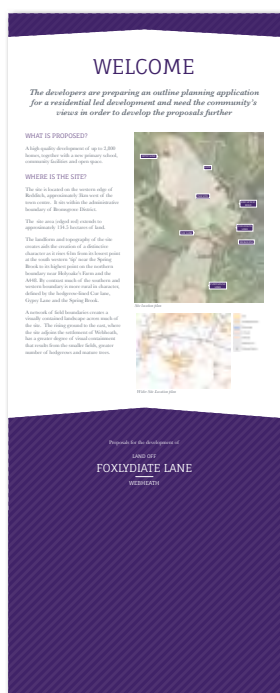
- » Pre-application meetings with Bromsgrove District Council and Redditch Borough Council;

» Pre-application meetings with technical and statutory stakeholders

» Public Exhibition - 28 and 29 January 2016 the applicants consulted with the local community by hosting a Public Exhibition and website in respect their emerging proposals for residential development.

» Public Exhibition - 26 February 2016 the applicants consulted with the local community by hosting a Public Exhibition with feedback from the January events in respect of the emerging proposals for residential development.

6.1.4. The full extent and outcomes from the programme of engagement is set out in the Statement of Consultation which supports the hybrid planning application.







### DRAFT MASTERPLAN

The draft illustrative masterplan shows how the site can be developed to create a high quality new scheme that responds to the existing context of the site.

Key features include:

- Up to 2,000 new dwellings
- 2,000 sqm of community space
- 12 green spaces
- New local centre
- Additional secondary schools
- Accessibility and inclusive design

Proposed for the development of  
**LAND OFF  
FOXLYDIATE LANE  
WIMBORNE**

### SOCIAL AND ECONOMIC IMPACT OF THE DEVELOPMENT

A number of key social and economic benefits will be generated as a result of the development.

Key benefits include:

- AFFORDABLE HOUSING:** 10% of new dwellings will be affordable.
- CONSTRUCTION JOBS:** 1,000 jobs during construction.
- COMMERCIAL EXPENDITURE:** £10 million per year.
- NEW HOMES BONUS:** 10% of new dwellings will be affordable.

Proposed for the development of  
**LAND OFF  
FOXLYDIATE LANE  
WIMBORNE**

### ACCESS AND MOVEMENT STRATEGY

Key features include:

- WALKING:** 10% of new dwellings will be within walking distance of a bus stop.
- CYCLING:** 10% of new dwellings will be within cycling distance of a bus stop.
- HIGHWAYS:** 10% of new dwellings will be within highway distance of a bus stop.

Proposed for the development of  
**LAND OFF  
FOXLYDIATE LANE  
WIMBORNE**

### GREEN INFRASTRUCTURE

A Green Infrastructure approach enables landscapes to deliver social, economic and environmental benefits simultaneously, and then looks at how those benefits can be multiplied by being connected to a wider network of spaces.

Key features include:

- 10% of new dwellings will be within green infrastructure distance of a bus stop.
- 10% of new dwellings will be within green infrastructure distance of a bus stop.
- 10% of new dwellings will be within green infrastructure distance of a bus stop.

Proposed for the development of  
**LAND OFF  
FOXLYDIATE LANE  
WIMBORNE**

### NEXT STEPS

Proposed for the development of  
**LAND OFF  
FOXLYDIATE LANE  
WIMBORNE**

## 6.2. EVALUATION

*Site, wider context assessment and involvement of the community and stakeholders have provided valuable information to shape and evolve the design proposals. This section sets out an evaluation of the proposals to show how they meet the economic, social and environmental roles set out in the NPPF.*

### ECONOMIC

6.2.1. ‘Contributing to building a strong, responsive and competitive economy’\*

#### Assessment

6.2.2. Studies and assessment work have been undertaken to identify economic benefits that the development will generate when it is delivered.

#### Involvement

6.2.3. Discussions have been undertaken with the LPA and the local community with regard to economic benefits that the development is likely to generate. This includes the Section 106 contribution that will be made as a result of the development coming forward which will help to enhance existing facilities in the local area.

#### Concept and Principles

6.2.4. A number of economic benefits will be generated as a result of the development. They include:

- » Additional labour force and economic output on completion.
- » A New Homes Bonus to the LPA and Worcestershire County Council.
- » New jobs created during construction.
- » Additional commercial expenditure which will be spent in the local economy.

### SOCIAL

6.2.5. ‘Supporting strong, vibrant and healthy communities’\*

#### Assessment

6.2.6. Studies and assessment work has been undertaken to identify key facilities that could be provided on site to aid the creation of a cohesive community.

#### Involvement

6.2.7. Discussions with the Local Authorities, key stakeholders and the public have identified the requirement for community facilities to be located on the site. This includes affordable housing, open space provision standards, walking and cycling routes and a central play facility, as discussed in detail with leisure officers. Design led discussions have also been undertaken with the LPA’s to ensure the provision of a scheme that will provide a safe, attractive and enjoyable place to live, nurturing the creation of a happy and healthy new community.





## ENVIRONMENTAL

### *Concept and Principles*

6.2.8. A key aspiration of the masterplan is to create a high quality and distinctive new place that brings benefit to both the new and existing community. The development has been shaped by existing green infrastructure; this forms a network of green corridors that vary in formality and function. They link to significant areas of new open space, providing plentiful exercise and recreation opportunities on the doorstep.

6.2.9. The provision of a co-located local centre (including health, community and retail uses) and first school aim to create a vibrant and active community hub which provides the opportunity for social interaction. These facilities have been located in the most central place within the development and are accessible by foot, bus, bike and car.

6.2.10. The development will include a mix of house types and tenures; the provision of up to 40% affordable housing and Starter Homes will help meet the housing needs of local people.

6.2.11. 'Contributing to protecting and enhancing our natural, built and historic environment'\*

### *Assessment*

6.2.12. Environmental and technical surveys have been undertaken for disciplines which include access and movement, landscape, ecology, drainage, heritage and archaeology, utilities and noise. The findings of these surveys have informed the masterplan and helped to ensure that it is responsive to important site assets.

### *Involvement*

6.2.13. Discussions on each of these technical disciplines have been undertaken with LPA Officers and key stakeholders as appropriate. A summary of the findings of technical studies were presented at the public exhibitions held in January and February 2016 with explanation provided with regard to how these technical studies had helped to shape the proposals.

6.2.14. The client team have had significant discussions with the LPA regarding heritage, with detailed studies, meetings and a number of site visits being undertaken to ascertain the extent of development in the central and northern parts of the site. These discussions have significantly changed some areas of the scheme; these changes are detailed further in section 6.3.

### *Concept and Principles*

6.2.15. The masterplan is responsive to important site assets. It retains these assets wherever possible and aims to enhance their setting. Existing green capital has formed an important shaping element of the masterplan and will be integrated with new areas of open space that allow for views, ecological habitats, SUDS, tree and hedgerow planting. Appropriate setbacks will be given to existing dwellings that adjoin the site boundary, with consideration given to how the amenity and privacy of these existing properties will be respected.

6.2.16. Detailed studies and discussions have informed the extent of development with regard to Lane House Farm and Hewell Grange.

*\*Quotes taken from the NPPF (Department for Communities and Local Government, 2012)*

## 6.3. DESIGN EVOLUTION

*The following pages show how the masterplan has evolved and describe different options that were explored during the design process, in collaboration with Redditch and Bromsgrove Local Planning Authorities and additional key stakeholders.*

### NOVEMBER 2014

6.3.1. This concept plan was presented in the Promotional Delivery Document, produced in November 2014. The document was submitted to the LPA's to present details of the emerging proposals and further support their decision to allocate the site for development. This plan was informed by technical surveys and the following design concepts;

- » Vehicular access points located on Birchfield Road, Foxlydiat Lane and Cur Lane.
- » Potential emergency access point located on Pumphouse Lane.
- » Establishment of primary and secondary movement routes and loops that aid permeability throughout the site.
- » First school, community building and local centre co-located within close proximity of the Birchfield Road access point to ensure the creation of an active and engaging new community hub that is accessible to all.
- » Areas for formal sport (pitches) located in the north and southern areas of the site.
- » Provision of a significant quantity of open space that accommodates new and existing planting, pedestrian and cycle routes, areas for play and drainage.
- » Provision of north – south green corridors that retain a route along the Spring Brook.



*Land Use Masterplan (LUMP) November 2014*



## SEPTEMBER 2015

6.3.2. A revised masterplan was produced in September 2015 to reflect further technical surveys and input. The following updates were made to the plan;

- » Inclusion of the community building within the local centre.
- » Re-alignment of northern movement loop to further aid accessibility.
- » Provision of additional/enlarged areas for attenuation.
- » Revised location of sports pitches to aid the creation of a central area for formal sport provision.



*LUMP September 2015*

## NOVEMBER 2015

6.3.3. The masterplan was revised to reflect discussions with the LPA planning, heritage and leisure Officers which focused on the following;

- » Quantum of sports pitches to be provided.
- » The creation of an accessible and centrally located formal sports area that provided formal play to cater to a range of ages and requirements.
- » Ensuring that the setting of Hewell Grange Conservation area and Lane House Farm is respected.

6.3.4. Therefore, a number of amendments were made to the masterplan;

- » All sports pitches are located within close proximity to each other (within 200m, less than a 5 minute walk).
- » Revision to the location of the local centre and first school to accommodate the revised arrangement of sports pitches. This revised location is also more accessible to all areas of the site.
- » Locating the first school on the western site edge (adjacent to Lane House Farm) aimed to ensure that no residential development was located in this area.
- » Development pulled back within the northern area of the site to ensure that there are no views to proposed development from Hewell Grange Conservation Area, in particular, the Walled Garden.
- » Development pulled back in the south western corner of the site to accommodate additional drainage areas and further enhance the setting of the Spring Brook.
- » Additional vehicular access point located on Foxlydiat Lane.
- » Further revisions to the alignment of the primary movement route in the northern area of the site to respond to the topography and the revised arrangement of sports pitches and attenuation areas.



LUMP November 2015



## DECEMBER 2015

6.3.5. Further revisions were made to the masterplan as a result of continued discussions with the Officers in relation to leisure and heritage, in particular with regard to the setting of Lane House Farm.

- » Local centre and school re-located near to the Birchfield Road vehicular access point and provision of public open space in the central western part of the site to respect the setting of Lane House Farm.
- » Reduction in the provision of sports pitches; 2 pitches were located this new area of POS.
- » Development pulled back in the central western area in accordance with important views from Lane House Farm, as identified on site visits that were attended by planning and heritage Officers and the client team.
- » Proposed downgrading and re-location of primary movement route where necessary to aid the creation of a parkland character.



LUMP December 2015

## DECEMBER/JANUARY 2016

6.3.6. A revised version of the masterplan was issued at the end of December 2015 to reflect an additional meeting with the LPA's to further discuss heritage and leisure elements of the scheme. These discussions included;

- » Removal of sports pitches and provision of a central area to accommodate formal play. This could include LEAP's, NEAP's an areas for an informal kick about and wheeled play. It was agreed that an area for formal play would also be included in the south western corner of the masterplan to ensure that play facilities were easily accessible to residents in this area.
- » Revised location of local centre and the first school to the west of the Hawthorn Pit, removing development from the western edge of the site that is adjacent to Lane House Farm. This location was seen as also being the most central location for access from all parts of the site. Co-locating these uses also enables key facilities to be access within a single trip.
- » Changing rooms and adjoining development block removed, helping to retain views from Lane House Farm.



LUMP December / January 2015



## MARCH 2016

6.3.7. A final version of the masterplan was produced in March 2016. This is the masterplan presented in the DAS.

6.3.8. The final masterplan incorporates amendments of a minor nature, including revisions to the Birchfield Road and Foxlydiat/Cur Lane junctions and the addition of a local centre plaza.

6.3.9. Revisions were also made to the development parcel south of the Pumphouse Lane site entrance to allow the existing PRow to be incorporate within the highway. This access point (formerly accommodating emergency vehicles only) could now also accommodate vehicles and a potential bus route.

6.3.10. Illustrative strategies relating to elements of the masterplan including access and movement, landscape, drainage and character are set out in sections 8 and 9 of the DAS.



*Concept Masterplan March 2016*

## 6.4. DESIGN PRINCIPLES

*Four key ideas underpin the proposals for Monarch Green. These ideas have been informed by the vision and an analysis of the site and existing local character. This aims to ensure the creation of an exciting, vibrant development that is site specific.*

6.4.1. These four ideas form a strong design framework for the proposals. They are explained below and illustrated on the plan presented opposite;

### 1. SIGNIFICANT GREEN INFRASTRUCTURE

6.4.2. The provision of a significant and connected green infrastructure is a key concept for the proposals. The development will be focused around an active and vibrant green spine comprising existing mature planting and a range of uses to cater for all. Spring Brook will form a unique and attractive backdrop for new riverside walks and spaces to relax, play and enjoy.

### 2. A NEW, THRIVING LOCAL CENTRE

6.4.3. At the heart of the new Monarch Green development will be a thriving new local centre comprising key retail, education and community facilities. It will provide a central focal space for the development and be easily accessed via all modes of transport by the new and existing community.

6.4.4. The local centre will be designed to encourage and facilitate social interaction through the provision of new facilities and spaces which people enjoy visiting.

### 3. EXCEPTIONALLY RESPONSIVE TO IMPORTANT SITE FEATURES

6.4.5. A comprehensive analysis of the site and surrounding area has highlighted important site features which are integral to shaping development and ensuring the creation of a distinctive and site specific character. These features have been embedded within the heart of the proposals and thus, the masterplan will respond to existing green capital, views, topography, desire lines and key surrounding land uses.

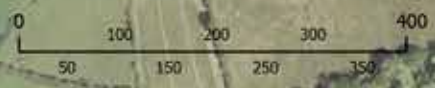
### 4. CREATING A BEAUTIFUL PLACE

6.4.6. Beauty is important and can take many forms. Garden Cities conjure up a certain architectural style in the mind's eye that is very English and very much of its time. The vision for Monarch Green does not dictate aesthetics; it believes that beauty should be evolve from the relationship of the development with nature, the rhythm, proportion and scale of its buildings and spaces and its attention to detail.





-  Primary Vehicular Access Point
-  Secondary Vehicular Access Point
-  Residential Development
-  Local Centre
-  Primary School
-  Public Open Space
-  Green Corridor
-  Area for Formal Play
-  Informal Parkland
-  Bridleway
-  Public Right of Way
-  Monarch's Way



*Design Principles plan*



## 6.5. DESIGN CONCEPT

*The concept masterplan presented opposite further evolves the design principles and shows how the proposals for Monarch Green could be realised.*

### GENEROUS GREEN INFRASTRUCTURE

- » The provision of a connected, attractive and high quality green infrastructure is a key asset of the proposals. Over 50 hectares of open space will be provided that includes recreational open space and parkland, walking and cycling routes, areas for play, existing tree and hedgerow planting, ecological habitats and drainage.
- » A new area for formal equipped play will be provided centrally within the site. It will cater for a range of uses and ages.
- » The proposals place a strong emphasis on wildlife habitat creation within areas of open space, in particular the Spring Brook corridor. Both blue and green infrastructure will be designed to enhance biodiversity and ecology wherever possible.

### CONNECTED

- » Primary vehicular access will be located at Birchfield Road.
- » Additional secondary vehicular access points will be located on Foxlydiate Lane, Cur Lane and Pumphouse Lane.
- » Existing pedestrian and cycle linkages within the site will be retained wherever possible and integrated with a new network of attractive and accessible routes. Pedestrian, cycle and bus linkages will reflect desire lines to key facilities and spaces within the site, Monarch's Way, Public Rights of Way, Sustrans Route 5, Webheath and Redditch.
- » The creation of a connected network of streets that have a clear hierarchy aids the creation of a legible and permeable development that is enjoyable to travel around.

### DISTINCTIVE

- » A truly unique place will be created which draws on existing important site features and positive design elements of Redditch and its surrounding settlements.
- » A distinctive character will be encouraged through the retention of views within the site and to the surrounding areas as appropriate. Existing landscape features within the site will be celebrated, with green capital retained and enhanced within new areas of open space.
- » The proposals seek to promote community pride and social interaction through the creation of a safe and accessible network of spaces and places which are enjoyable destinations to visit.

### A NEW GARDEN NEIGHBOURHOOD

- » The development will provide new retail, education, health and community facilities that will form an exciting focal space for new and existing community. These facilities will be easily accessible to new residents via foot, bus and bike linkages.
- » The establishment of a strong landscape framework aims to ensure a generosity of open space within the development, further enhanced by new strategic and street tree planting.
- » A high quality place will be created, underpinned by garden city and best practice urban design principles. The structure of development has been inspired by the retention of green infrastructure, the provision of a connected network of streets and creation of a happy, safe place where people aspire to live.





-  Primary Vehicular Access Point
-  Secondary Vehicular Access Point
-  Residential Development
-  Local Centre
-  Primary School
-  Public Open Space
-  Existing Tree/Hedges/ow Planting
-  Proposed Tree/Hedges/ow Planting
-  Area for Formal Play
-  Allotments/Grow Patches
-  Areas for attenuation/drainage
-  Bridleway
-  Public Right of Way
-  Monarch's Way
-  Proposed Cycle Route
-  Proposed Informal Pedestrian Route
-  Possible future development within allocation area

*Design Concept plan*







*Chapter 7:*

# ILLUSTRATING THE QUALITY OF DESIGN: SUPPORTING STRATEGIES

## 7.1. SUSTAINABILITY AND ENERGY

***A Sustainability and Climate Change assessment has been produced and the Proposed Development responds positively to the three pillars of sustainable development; Economic, Social & Economic Roles.***

7.1.1. The Proposed Development will provide employment during its construction, and once complete, will provide employment opportunities. It will stimulate and support both the local and regional economies through demand for services and providing employees to employers. The proposal will help to sustain the vitality and viability of Webheath area, by providing homes for economically active residents. Accordingly, the Site is considered to be consistent with the economic role as promoted within the NPPF.

7.1.2. The scheme would provide a wide range of high quality homes to cater for a variety of local housing needs. Further to this, the proposal also encompasses various supporting social infrastructure for the health and wellbeing of both existing and new residents. The location of the Site is shown to be in close proximity to existing key local services in Redditch. With these matters taken into consideration, the Site is considered to be consistent with the social role promoted within the NPPF.

7.1.3. The masterplan has incorporated various environmental considerations in order to limit adverse effects on the existing environment, improving it where possible. The Proposed Development also includes mitigation of significant impacts on the environment. The Proposed Development is located where it can be accessed by public transport. In compliance with the emerging District Plan policy, the design of the development includes sustainable construction measures to achieve BREEAM ‘very good’ standard for non-residential building. However, in relation to residential development, the Government has now superseded the Code for Sustainable Housing with Building Regulations. As such, the design of the Proposed Development has been developed with reference to the Building Regulation to ensure that this aspect of the proposal is sustainable. The proposals are therefore considered to be environmentally sustainable and consistent with the environmental role promoted within the NPPF.

7.1.4. The Proposed Development has been assessed against national and local sustainability criteria. The report concludes that the Proposed Development constitutes a sustainable development which will be resilient to future climate change through appropriate design measures.



*Indicative image*





## 7.2. ACCESS AND MOVEMENT

*The outcome of the Transport Assessment is a comprehensive set of strategies to improve public transport connectivity, walking and cycling routes, the capacity and safety of the road network and access to the site. This is complimented by a travel behaviour change programme, promoted by the County Council, and contained within a Travel Plan document.*

### PEDESTRIAN AND CYCLE STRATEGY

#### *Within the site*

7.2.1. A comprehensive set of walk and cycle routes will be provided within the site. Each road will be furnished with wide footways, and on the primary roads they will be separated from traffic by a wide landscaped verge. Cycle routes will be provided from each development parcel to the local area play, the proposed school and local shops.

7.2.2. Public Rights of Way through the site will be made more accessible and, where necessary, diverted along safe and attractive routes.

#### *On the local network*

7.2.3. Sustrans and Worcestershire County Council have agreed that the National Cycle Network can be diverted through the site, along segregated routes, and connect the site with Redditch Town Centre.

7.2.4. The County Council are being asked to consider opportunities to reduce the speed of traffic on Cur Lane to encourage the use of it by joggers, horse riders and cyclists.

7.2.5. Formal pedestrian and cycle crossings will be provided on Foxlydiate Lane and Birchfield Road, to provide connections with neighbouring communities. Routes within Webheath will be see improvement with better wayfinding and surfacing.

7.2.6. The route to the Town Centre will be enhanced with a segregated alternative to Bromsgrove Road through the underpass via Batchley.

### BUS STRATEGY

7.2.7. Public transport will play an important part in encouraging sustainable travel behaviour and it is therefore proposed to re-route existing bus service 47/48 through the development site and improve its timetable. This would provide a service to Redditch bus station, for the town centre and connection with other bus services and the train station, and to Headless Cross and Alexandra Hospital.

7.2.8. The route would pass through all parts of the development, and there is the opportunity to provide a better connection to properties on the Webheath ADR site. Walk routes to/from bus stops would be provided to create safe, secure access to the re-routed service. Passenger shelters and real-time information displays would be provided at the more important bus stops. Almost all houses will be within 250m of the potential route. This may vary, and it depends upon the viability of the service and reliability of the route elsewhere.

7.2.9. The route will provide public transport connectivity within Redditch, seven days per week throughout core travel periods. It will be with a six minute walk of dwellings and serve the local centre. It will provide an excellent connection with the Cross City Line train service and thus to/from the whole Metropolitan area. Furthermore it be an improved service for existing Webheath neighbourhood, with an extended service period, more frequent departures and the potential for local route modifications.





-  Primary Vehicular Access
-  Secondary Vehicular Access
-  Public Rights of Way (with associated pedestrian access to site)
-  Bridleway (with associated pedestrian access to site)
-  Proposed Bus Route
-  Potential Future Bus Route
-  Proposed Cycle Route
-  Proposed Informal Pedestrian Route
-  Bridleway
-  Public Right of Way
-  Monarch's Way



## VEHICULAR ACCESS

7.2.10. The site will take its principal access from Birchfield Road at its junction with the A448 Bromsgrove Highway. This will take the form of a new signalised junction, and will have the capacity to accommodate the traffic forecast from the development and background growth. Alterations will also be made to Birchfield Road where it meets the A448 slip road, to ensure the safety of emerging vehicles.

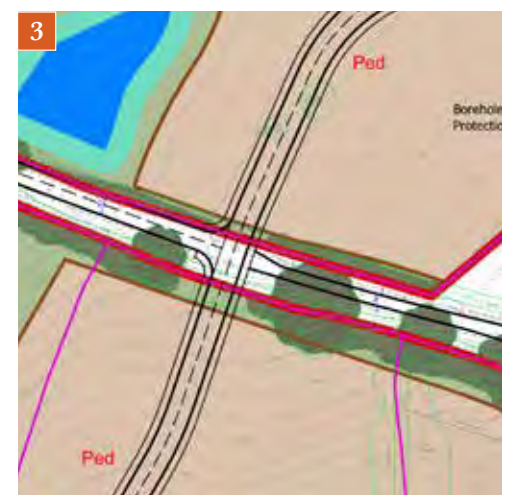
7.2.11. The principal access will be extended into the site to form a Boulevard from which all parts of the development will gain access. This will then connect to the Foxlydiate Lane roundabout in place of Cur Lane, which will be closed at this section.

7.2.12. Cur Lane will be closed between the Severn Trent Water borehole station and Foxlydiate Lane. It will be reconnected to the Boulevard via a minor road within the development.

7.2.13. A contained number of properties will also be accessed from Pumphouse Lane. This will form a minor route to the local network and may also be used as a bus connection between the site and the Webheath ADR. Marginal road widening and remedial maintenance works may be required on Pumphouse Lane itself.

7.2.14. A limited number of properties will be accessed from Foxlydiate Lane. This will form a minor route to the local network. The access is located where visibility is optimal and can be achieved within standards with the necessary splays. Worcestershire County Council have also been asked to consider a traffic management scheme for Foxlydiate Lane to discourage rat-running and encourage existing and future traffic to use the Boulevard through the site.

1







## OFF SITE HIGHWAYS

7.2.15. The capacity of the junctions highlighted on the plan below have been considered in detail based upon a traffic impact threshold agreed with WCC.

7.2.16. The results of the BARHAM model and more detailed assessments have revealed that most local junctions within the vicinity of the site continue to operate within capacity and it is concluded that in most cases no further analysis will be required. The impact on the Birchfield Road junction with A448 Bromsgrove Highway is significant, as it forms the principal access to the scheme, and the mitigation requirements are described below.

7.2.17. The traffic model recognises that junctions further afield in Redditch and Bromsgrove (on the A38 for instance) suffer from the cumulative impact of this and other development schemes. These locations have been specified in the Infrastructure Development Plan (supporting evidence formed in support of the Local Plan Examination) and the developer will work with Worcestershire County Council to identify a commensurate financial contribution towards these works.

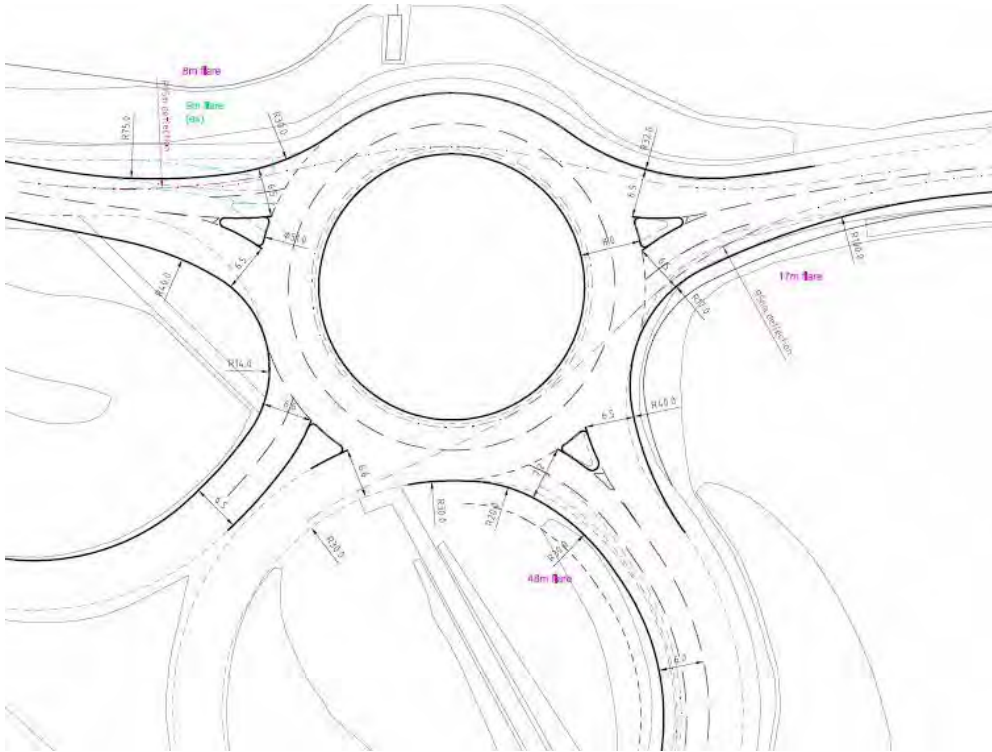
7.2.18. Junction capacity tests indicates that the Brockhill Drive roundabout junction with Hewell Lane experiences increased queuing and delay as a result of traffic generated by the proposed development. To mitigate the impact it is proposed to enlarge the roundabout as indicated on the plan opposite (top right) to allow more vehicles to pass through it.

7.2.19. Capacity tests also indicated that the Birchfield Road priority junction with the A448 slip road would operate with significant queues and delays. Therefore it is proposed to provide a ghost-island right turn lane into Birchfield Road to allow right turning traffic to queue without blocking ahead movements to the A448. Furthermore the right turn from Birchfield Road has been restricted, both to increase capacity and to overcome safety concerns raised by the community. This will be enforced by a physical island on Birchfield Road. Alternative routes to the A448 westbound will be available via the development site or by joining the A448 at Windmill Drive.



*Junction plan*





*Proposed junction design – Brockhill Drive roundabout junction with Hewell Lane*



*Proposed junction design – Birchfield Road*

## 7.3. STREET HIERARCHY AND CAR PARKING

*A distinctive hierarchy of street types is proposed for the development based on the principles of Manual for Streets. Each street will provide an attractive place to live alongside, or to pass through and enjoy.*

7.3.1. The categories of street will each have a different character and role within the scheme, the function of each is set out on the street hierarchy plan, presented opposite.

7.3.2. The development will incorporate the following street types;

- » Boulevard
- » Local Centre Plaza
- » Avenue
- » Street
- » Green Street
- » Community Street
- » Private Drive

7.3.3. The principles function of streets is explained and set out below. Each of these principles will be used across the site to shape the design of the scheme.

### **Place**

7.3.4. This defines the character and role of the street and elements that will make it distinctive and/or characteristic. The sense of place will be informed by a number of factors, including the buildings defining the space, the degree of enclosure, street trees, surface materials and function.

### **Movement**

7.3.5. These principles explain the movement and function of the street in terms of flows and the priority given to different modes of travel. A key objective of the movement strategy is to prioritise pedestrian and cycle movement wherever possible.

### **Access**

7.3.6. This describes how users will access buildings from the street. Where possible, the objective is to provide building frontages that are directly accessible on foot, with vehicle parking close to the main entrance of the property.

### **Parking**








7.3.7. This principle explains how parking for both visitors and residents is conveniently accommodated within the street scene and easily accessible from dwellings.

### **Utilities**

7.3.8. This explains the location of utilities within each street type.





-  Boulevard
-  Local Centre Plaza
-  Avenue
-  Street
-  Green Street
-  Community Street
-  Private Drives

Street Hierarchy plan

## BOULEVARD

### *Place*

7.3.9. The boulevard will be distinctive and attractive route through the site that links Birchfield Road with Cur Lane. Its design will reflect the importance of it functioning as the primary movement route for the development for all modes of transport. The route will incorporate verges and tree planting on both sides of the street.

### *Movement*

7.3.10. The boulevard will accommodate the highest flow of vehicles within the site. Its route through the development has been influenced by the topography and the establishment of connections to key facilities and the network of secondary and tertiary streets.

7.3.11. The design of the Boulevard will aim to prioritise pedestrian and cycle movement, with the provision of a 5 metre off road shared pedestrian and cycle route and vehicular speeds restricted to 20mph wherever possible.

### *Access*

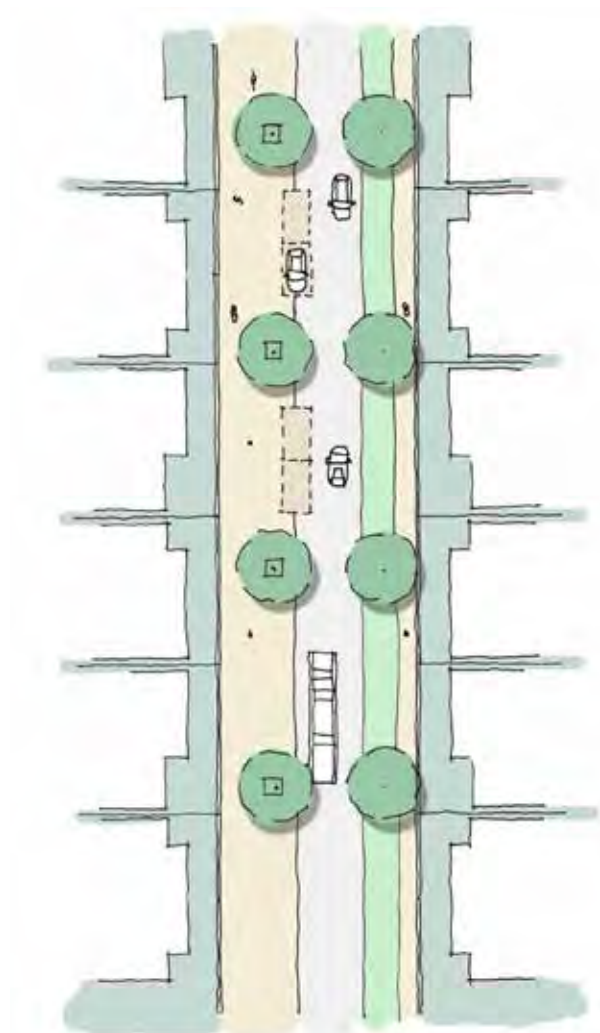
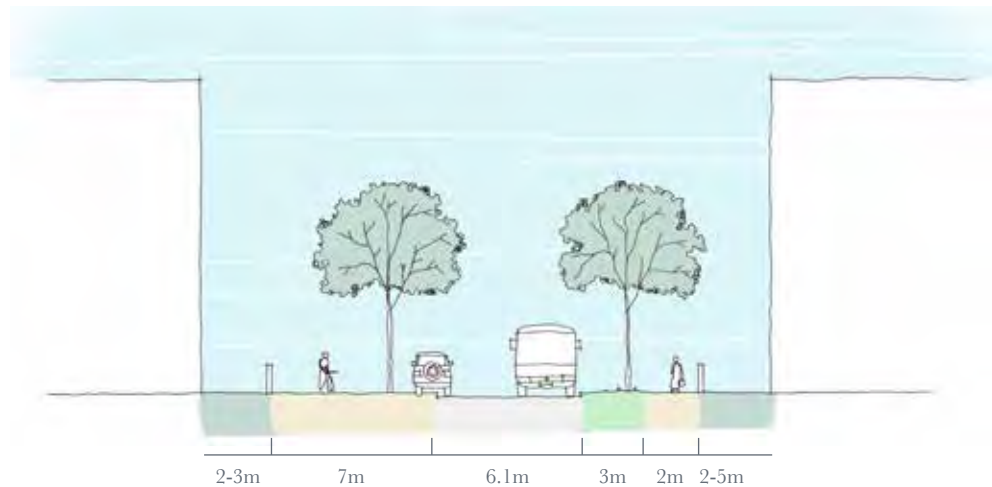
7.3.12. Direct access to dwellings will not be provided along the boulevard for vehicles.

### *Parking*

7.3.13. On plot parking will be provided for dwellings that is accessed via the secondary and tertiary network. Visitor parking will be provided at intervals along the street, as per Council policy.

### *Utilities*

7.3.14. Servicing will be accommodated beneath the highway and footway; it will be located to avoid conflict with tree planting.

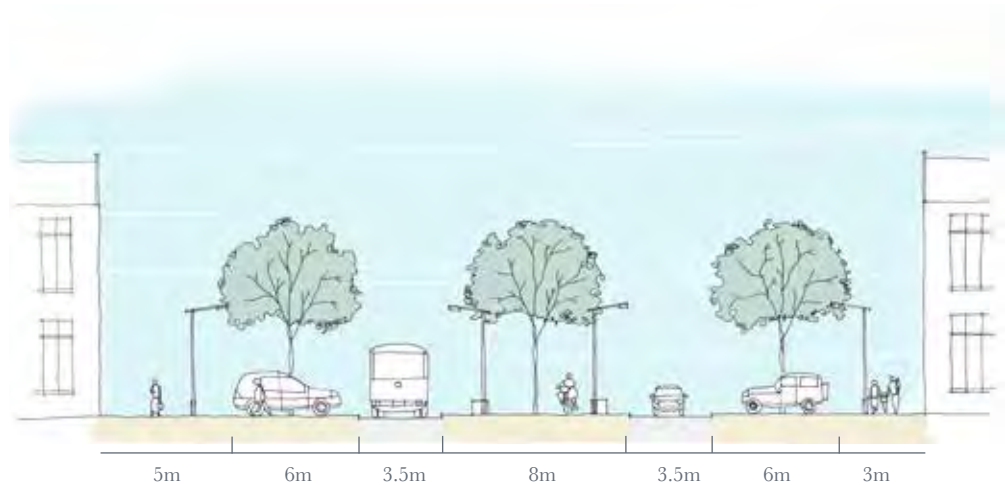




## LOCAL CENTRE PLAZA

### *Place*

7.3.15. The local centre plaza will provide a focal space and exciting new community hub for the development. It will be designed to function as a place for people to move through or stay and enjoy. Consideration will be given to the provision of tree planting and design of the public realm to ensure the creation of a high quality space that enables safe trips to be made to key facilities via all modes of transport.



### *Movement*

7.3.16. The local centre plaza will accommodate high flows of movement within the site and will be linked to the boulevard to the north and south. Low vehicular speeds will be encouraged in this area through the use of surface treatments, centrally located tree planting and deflections in movement.

### *Access*

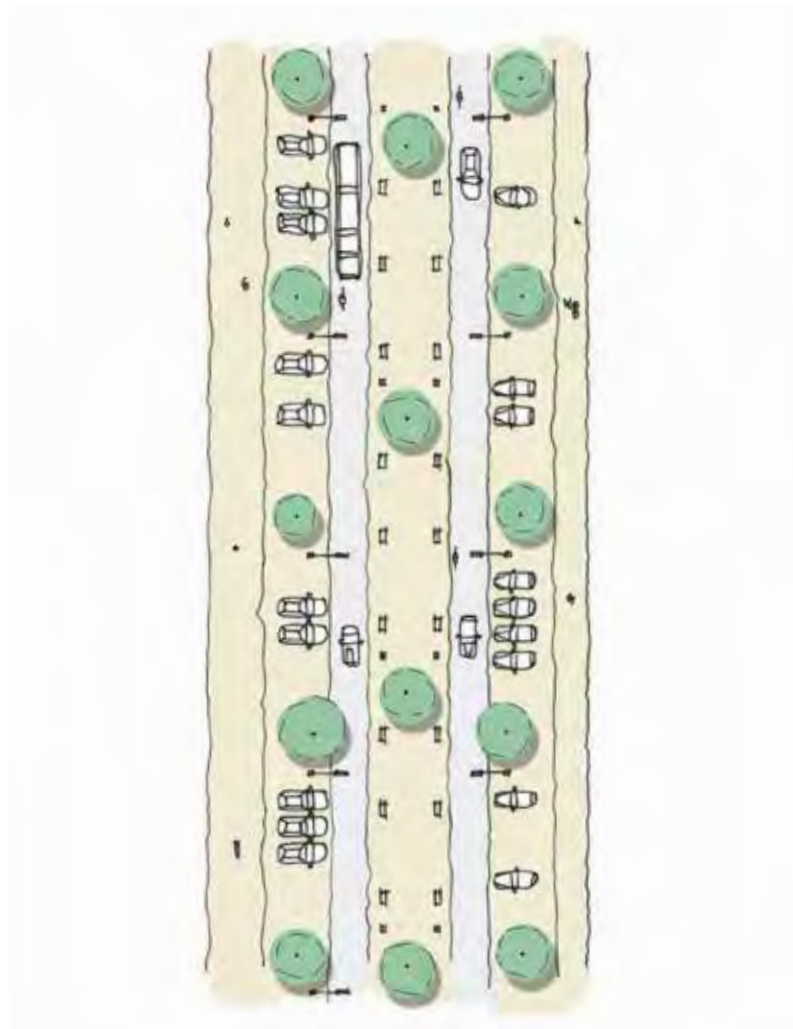
7.3.17. Direct access to dwellings will not be provided along the local centre plaza for vehicles.

### *Parking*

7.3.18. Visitor parking will be provided within the plaza space. Additional parking will be located within the local centre to serve retail, health and community facilities.

### *Utilities*

7.3.19. Servicing will be accommodated beneath the highway and footway; it will be located to avoid conflict with tree planting.



## AVENUES

### *Place*

7.3.20. Avenues will have a formal and green character, incorporating tree planting and verges on one side of the street. There will be some variations in character along avenues depending on their location within the development.

### *Movement*

7.3.21. Avenues aid the creation of movement 'loops' within the site, facilitating easy and direct movement around the site for vehicles, pedestrians and cyclists.

### *Access*

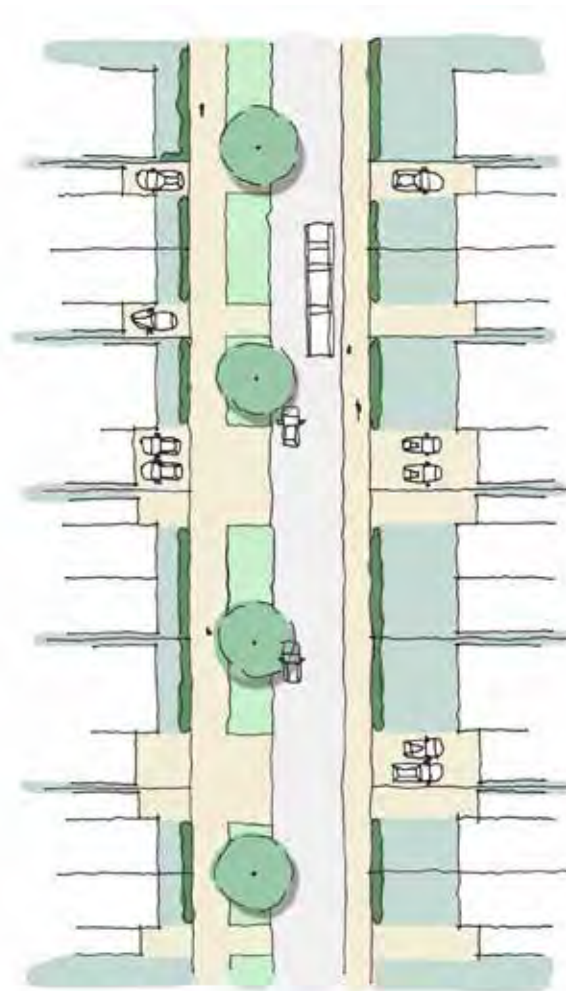
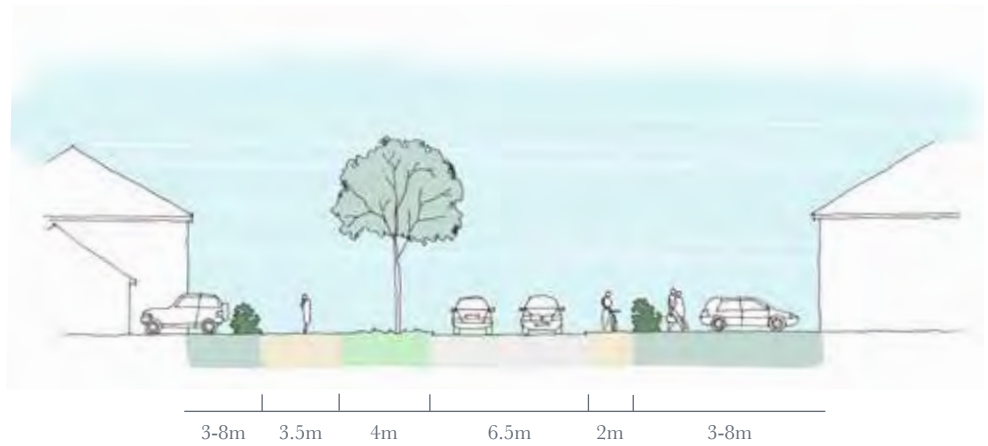
7.3.22. Avenues will provide direct frontage access to dwellings.

### *Parking*

7.3.23. On plot parking will be provided for dwellings. Visitor parking will be provided at intervals along the street, as per Council policy.

### *Utilities*

7.3.24. Servicing will be accommodated beneath the highway and footway. It will be located to avoid conflict with tree planting.





## STREET AND GREEN STREET

### Place

7.3.25. Streets will have a less formal character than the avenue and form part of the tertiary movement network. A variation of this street type; Green Streets, could be used in lower density areas of development that have a more rural and informal character.

### Movement

7.3.26. These routes will be designed to encourage low traffic speeds, with pedestrian footways provided either side of the highway. Both street types maintain a sufficient width to ensure safe passage for cycle movement.

### Access

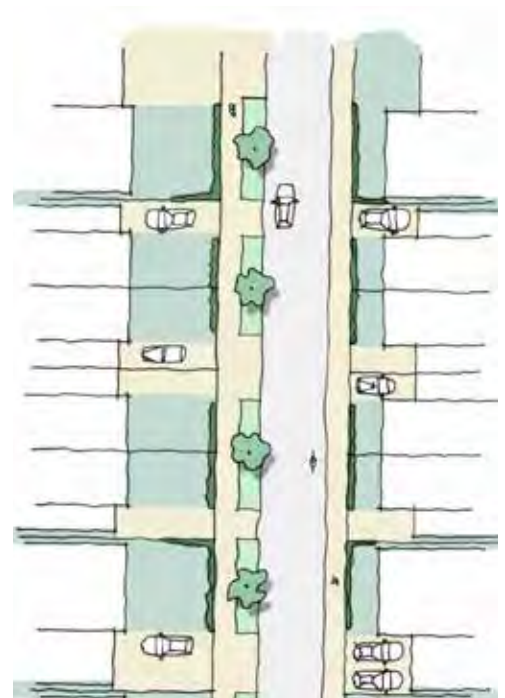
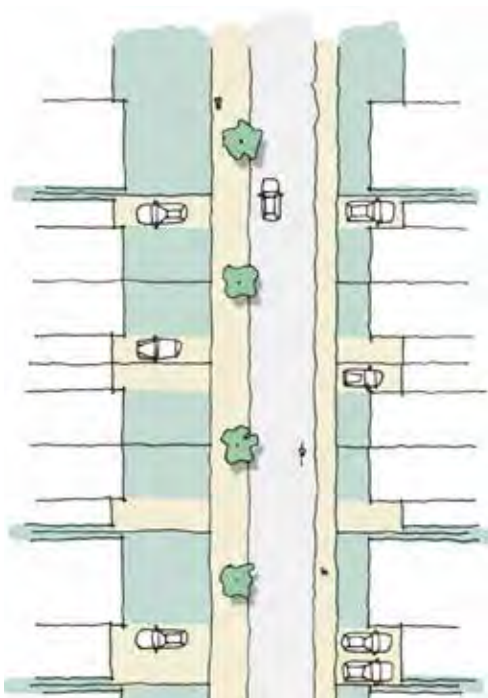
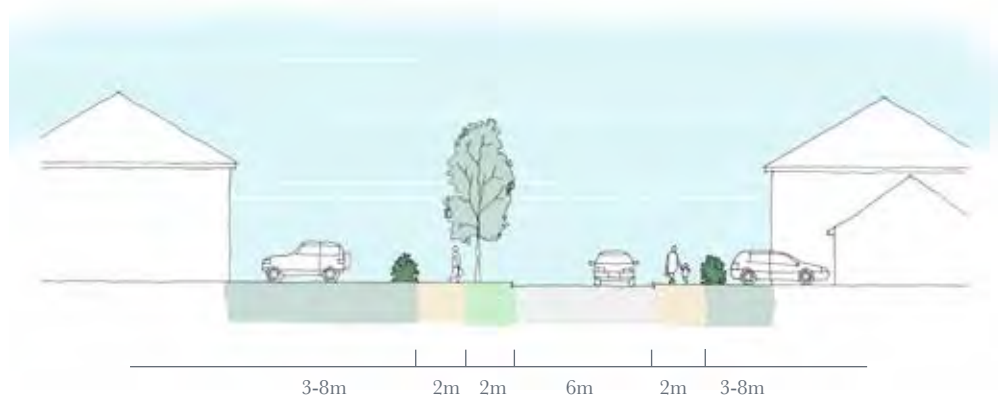
7.3.27. Direct frontage access to dwellings will be provided along the length of this typology.

### Parking

7.3.28. Car parking will be provided on plot with visitor parking provided on street, as necessary.

### Utilities

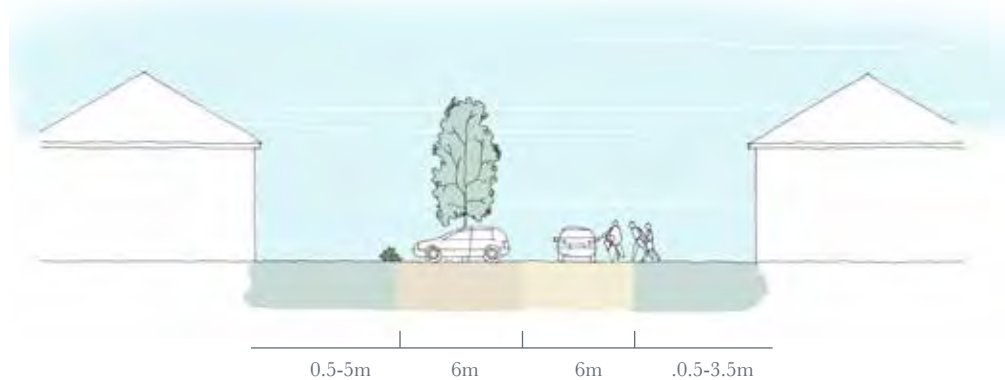
7.3.29. Servicing will be accommodated beneath the highway and footway.



## COMMUNITY STREET

### Place

7.3.30. These shared surface streets are intended to be important community places within the development. They are designed as places for people to walk, cycle and interact with neighbours. Their design seeks to encourage very low vehicle speeds with ‘ownership’ of the spaces belonging to the pedestrian and the use of deflections where appropriate.



### Movement

7.3.31. This street type will accommodate low traffic flows to allow for a safe interaction of transport modes, encouraged by the use of a shared surface treatment and a pedestrian/cycle priority.

### Access

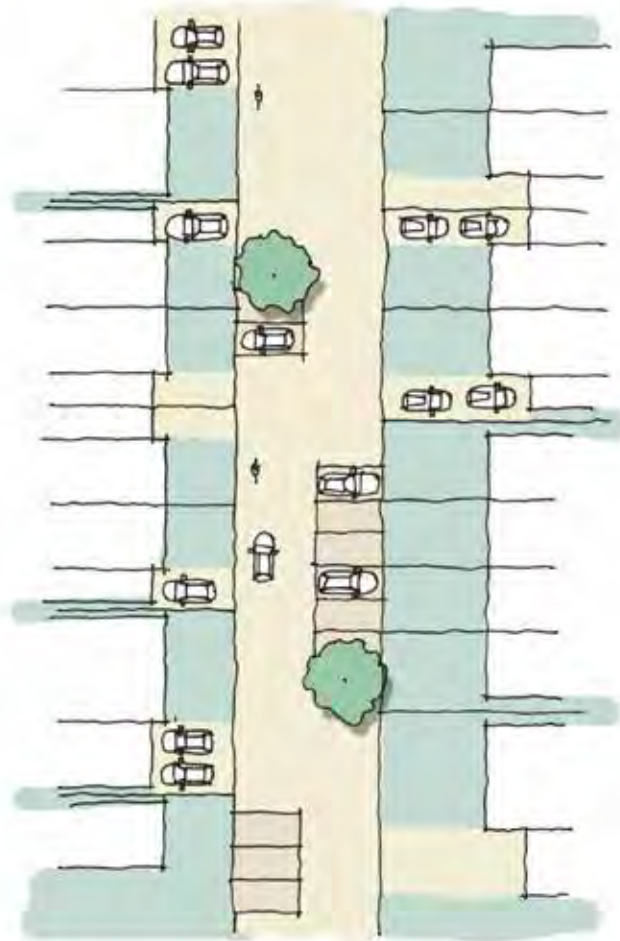
7.3.32. Community streets will provide direct access to dwellings.

### Parking

7.3.33. Parking for residents and visitors is accommodated within the design of the street; it will not act as an impediment to pedestrian and cycle movement. Parking will be integrated with tree planting to ensure the creation of an attractive street scene.

### Utilities

7.3.34. Servicing will be located to avoid conflict with tree planting; drainage is to pipe and gully.





## PRIVATE DRIVE

### *Place*

7.3.35. Private drives are informal, often located alongside open space. The low levels of traffic allow the carriageway to be a shared space between pedestrians, cyclists and vehicles.

### *Movement*

7.3.36. These street types will have low traffic levels and have pedestrian/cycle priority.

### *Access*

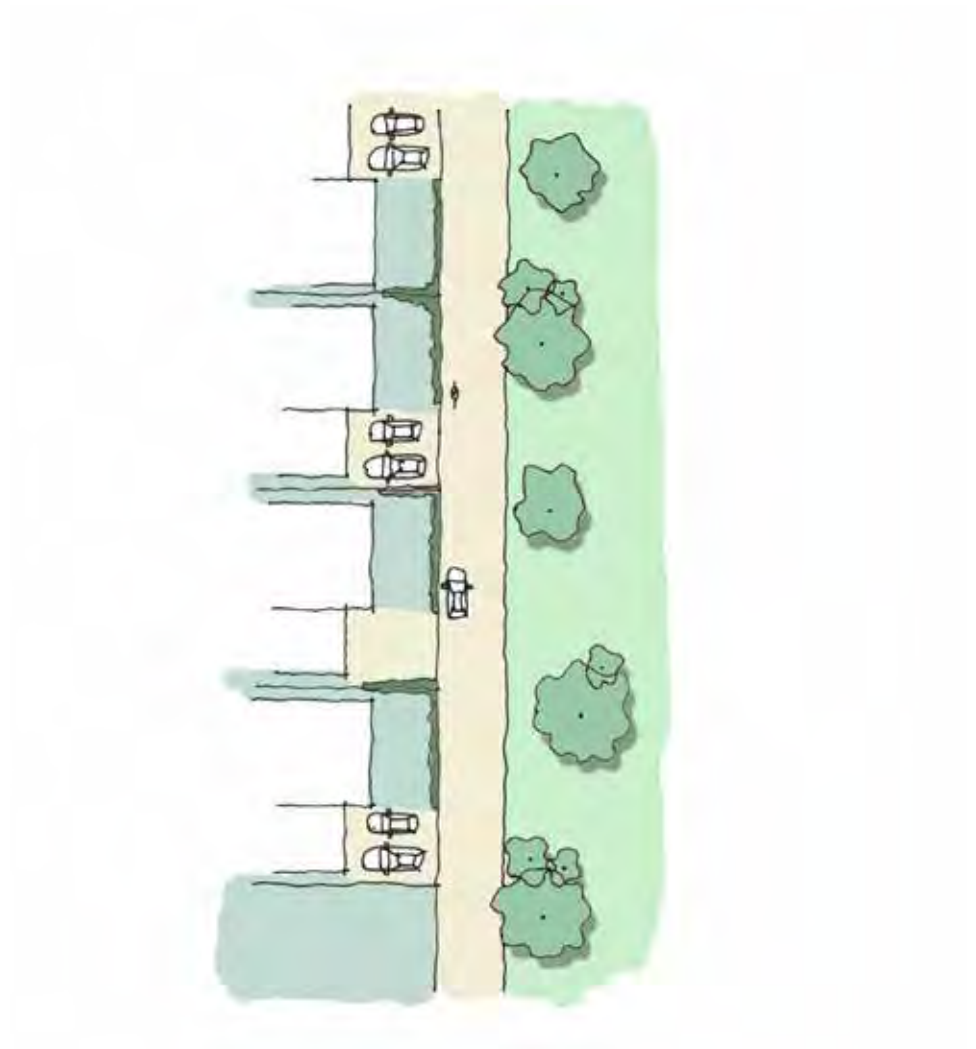
7.3.37. Private drives will provide direct access to dwellings.

### *Parking*

7.3.38. Each dwelling will have sufficient on-plot parking. Visitor parking will be provided informally within the street, where necessary.

### *Utilities*

7.3.39. A 0.5m strip for services and drainage will be provided.



## CAR PARKING

7.3.40. This section details a set of principles that could be used to ensure that car parking is appropriately and attractively accommodated within the street-scene.

7.3.41. The overall objective is to create streets which are attractive and functional places for pedestrians, cyclists and personal transport in an environment where car parking is accommodated conveniently on plot or on street near to the front door.

### *Car Parking Principles*

7.3.42. The following principles shall be applied wherever possible;

- » The quantum and location of resident and visitor car parking shall be discussed with the LPA.
- » Residents parking should be provided on-plot or on-street in a location that is convenient and overlooked.
- » Visitor parking will be provided at the appropriate ratio in accessible locations, usually on street.
- » Parking shall be designed to be as unobtrusive as possible by providing screening such as planting or hedges where appropriate.
- » Rear parking courts should only be used in exceptional circumstances; where used they should be small and overlooked.

- » Appropriate levels of disabled parking spaces will be provided in key locations.

7.3.43. The car parking arrangements presented in this section are considered appropriate for allocated parking. They seek to provide an appropriate amount of parking in accessible and convenient locations which are overlooked by dwellings.

### *Cycle Parking Principles*

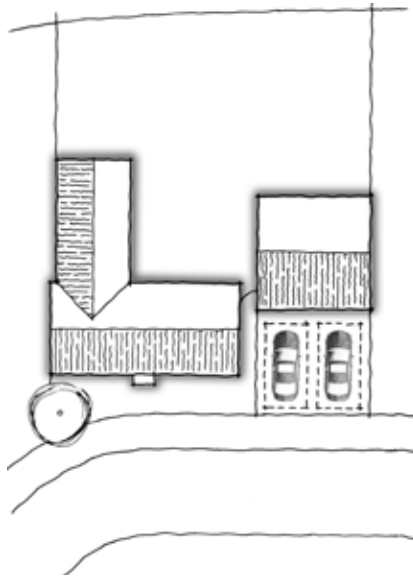
7.3.44. The following principles shall be applied wherever possible;

- » Cycle parking should be good quality and placed in prominent locations to help increase sustainable travel choices.
- » Cycle parking spaces for individual dwellings should be provided within the curtilage of the residential dwelling.
- » For flats cycle parking should be provided as a secure communal facility.



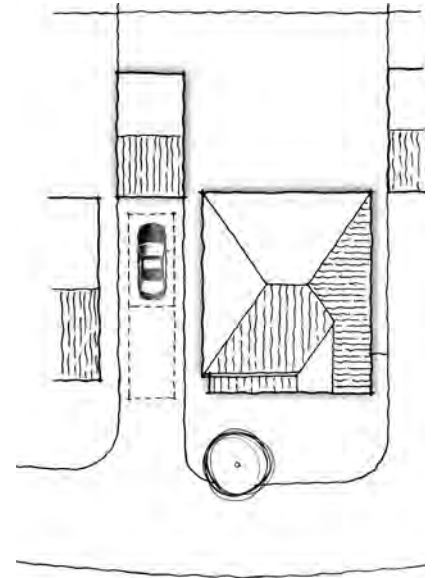
***On plot detached dwelling Option 1***

- » 2 on-plot parking spaces with hard surface area as well as double garage
- » Spaces overlooked by windows on front and side of property
- » Front garden softens visual impact



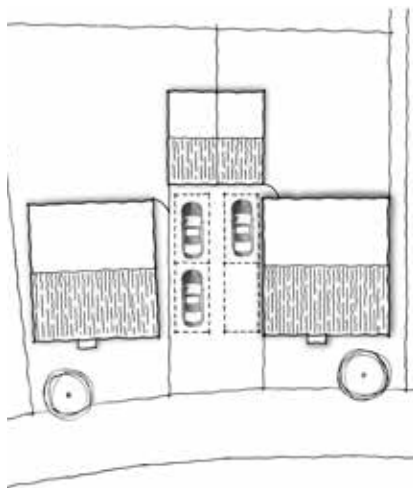
***On plot detached dwelling Option 3***

- » 2 on-plot parking spaces with hard surface area as well as single garage
- » Spaces overlooked by windows on side of property
- » Front garden softens visual impact
- » No visual impact, all cars are hidden on the side of properties



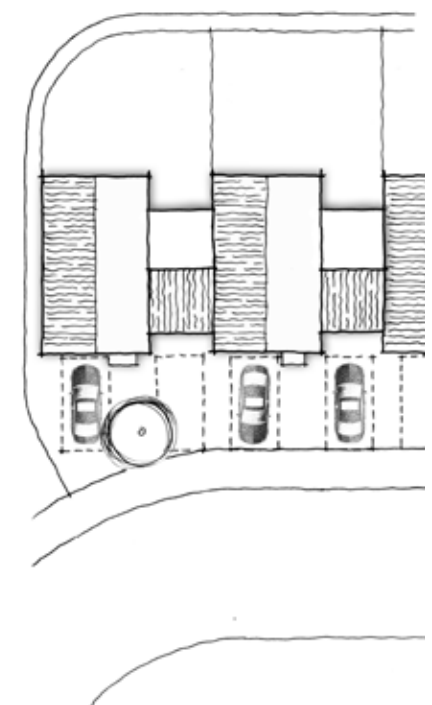
***On plot detached dwelling Option 2***

- » 2 on-plot parking spaces with hard surface area as well as single garage
- » Spaces overlooked by windows on side of property
- » Front garden softens visual impact
- » No visual impact, all cars are hidden on the side of properties



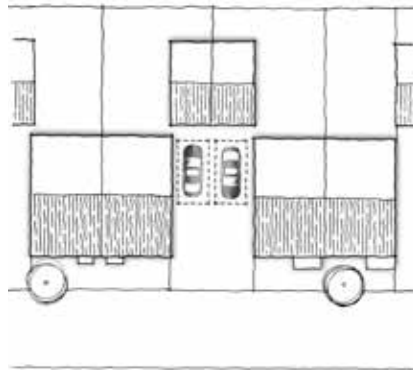
***On plot linked detached dwelling Option 4***

- » 2 on-plot parking spaces with hard surface area as well as single garage
- » Spaces overlooked by windows on front of property
- » Street tree planting softens visual impact



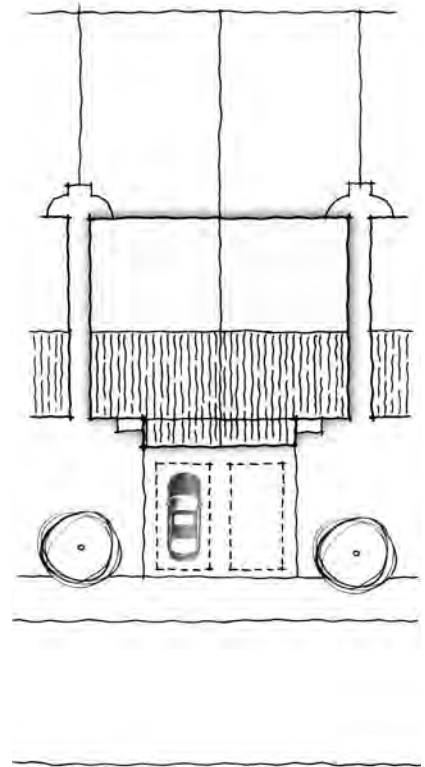
***On plot to the side of semi-detached dwelling Option 5***

- » 1 on-plot parking space with hard surface area as well as single garage
- » Spaces overlooked by windows on side of property
- » Front garden softens visual impact
- » No visual impact, all cars are hidden on the side of properties



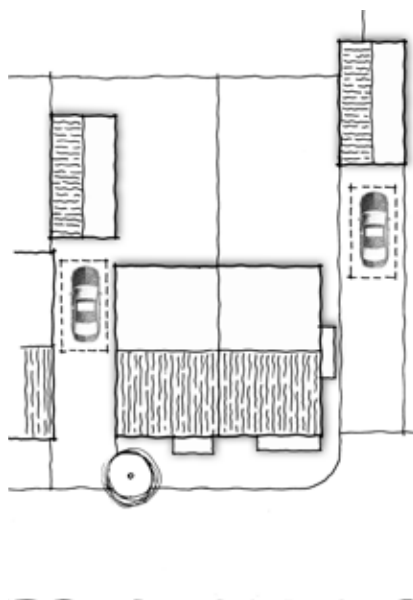
***On plot to the front of semi detached dwelling Option 7***

- » 1 on-plot parking space with hard surface area as well as single integral garage
- » Spaces overlooked by windows on front of property
- » Street tree planting softens visual impact



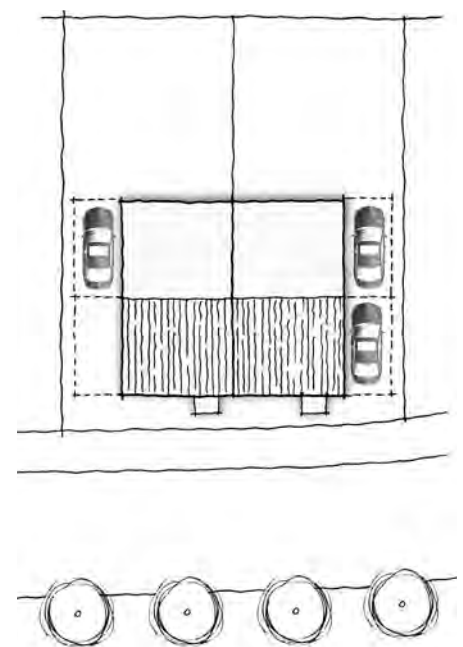
***On plot to the side of semi-detached dwelling Option 6***

- » 1 on-plot parking space with hard surface area as well as single garage
- » Spaces overlooked by windows on side of property
- » Front garden softens visual impact
- » No visual impact, all cars are hidden on the side of properties



***On plot to the side of semi-detached dwelling Option 8***

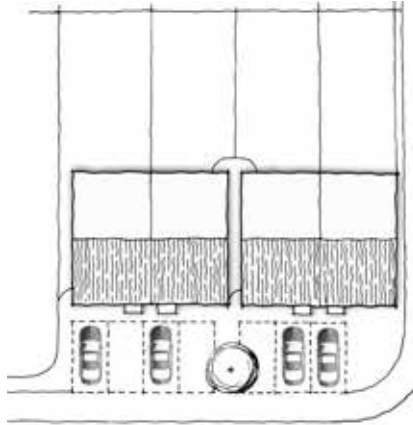
- » 2 on-plot parking spaces with hard surface area
- » Spaces overlooked by windows on side of property
- » Front garden softens visual impact
- » No visual impact, all cars are hidden on the side of properties





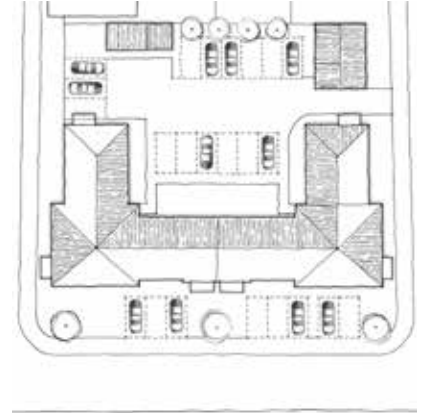
*On plot to the front of semi-detached dwelling*  
**Option 9**

- » 2 on-plot parking spaces with hard surface area
- » Spaces overlooked by windows on front of property
- » Street tree planting softens visual impact



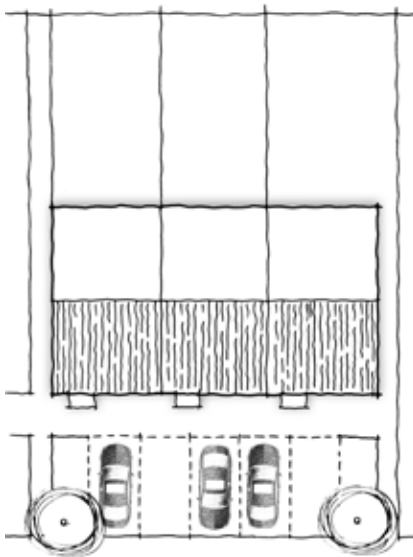
*On plot to the front and rear of apartments*  
**Option 11**

- » 1 on-plot parking space with hard surface area and single garage
- » Spaces overlooked by windows on front and rear of property
- » Tree planting softens visual impact



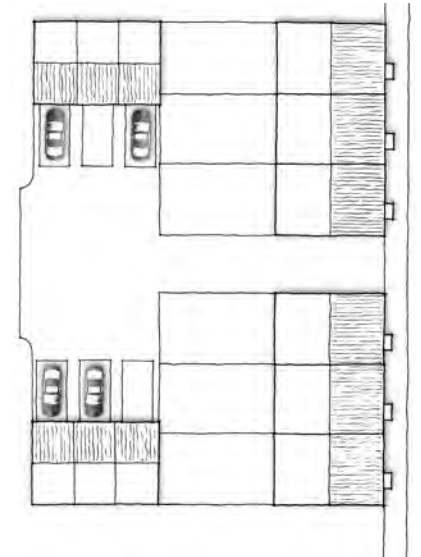
*On plot to the front of terraced dwelling*  
**Option 10**

- » Up to 2 on-plot parking spaces with hard surface area
- » Spaces overlooked by windows on front of property
- » Street tree planting and planted raised beds soften visual impact



*Parking Courtyard*  
**Option 12**

- » Use of this scenario should be restricted wherever possible
- » Up to 2 on-plot parking spaces per dwellings
- » Minimal visual impact, car parking is screened by dwellings
- » Layout of courtyards to vary according to location



## 7.4. THE LANDSCAPE MASTERPLAN

### LANDSCAPE STRATEGY

#### *The Objective*

7.4.1. The landscape Strategy for the new development has the clear objective to realise to the full all the advantages already present in the landscape of the Site and its immediate setting. The object is to craft a new landscape that forms a strong, local and highly successful response to the place. This over-arching objective sets out a clear requirement for landscape strategies that will deliver the opportunities for a memorable and enduring scheme. These strategies will make the most of the latent potential that is present in the landform; the view points and vistas; the connectivity with the countryside; and the links with the land and the history of the place.

#### *Scale in the Landscape*

7.4.2. The objective for the landscape of the new public places in the development: the parks and small green spaces, is to make a creative response in the new landscape that makes a direct relationship with the changing scale present in the existing landscape. This change of scale is important. It exists and ranges from the visible setting made by the expansive views over the countryside of Worcestershire from the high ground between Gypsy Land and the A448, to the near intimate small scale of the land south of Cur Lane near the Spring Brook.

7.4.3. One of the important threads within the Landscape Strategy is the creation of new public spaces within the development that will realise these ‘assets’ in the variety of the landscape scale. For example open space has been placed over the northern high ground of the Site that will have a parkland character, responding to the expansive landscape. The rising ground will sit within limits that already exist: in the hedgerows; the copse; and the shape of the landform. The parkland will be open and the views from numerous viewpoints in the public space, will be publically accessible for the first time. A counterpoint to this will be the connected network of small scale green spaces along the low land near the Spring Brook between Pumphouse Lane and Cur Lane.

7.4.4. The landscape design will create public spaces that are formed to encourage the enjoyment of being outdoors but not far from home and yet in an environment that has the nature of the place as its main character driver.

#### *Landscape Identity and Character*

7.4.5. The LVIA work records the distinctive change in the landscape character across the Site from the ‘Wooded Estatelands’ Landscape Character Type south to the ‘Principal Timbered Farmlands’. Landscape strategies will look to respect and strengthen the character defining traits identified in the landscape character studies. Consequently the open space to the north will have the qualities and character of estate parkland. The open space along the Spring Brook will be designed as a response to the stream-side tree cover and a stronger pattern of hedgerows.








**Boundaries and Rights of Way**






— Site Boundary

**EXISTING ELEMENTS**

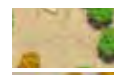





-  PRoW
-  Existing Trees
-  Existing Hedge

**PROPOSED ELEMENTS**




**Planting**

-  Proposed Trees
-  Proposed Hedge
-  Wetland Planting
-  Meadow Grassland
-  Amenity Grass



**SURFACING AND PAVING**

-  Enhanced Highway Paving
-  Enhanced Pedestrian Paving
-  Crushed Stone Surfacing
-  Cycle/Footpaths - Bitmac
-  Footpath - Crushed Stone
-  Informal Path - Worn/Mown Grass

**WATERBODIES**

-  Attenuation ponds & basins
-  Watercourse
-  Boardwalk/Bridge

**PLAYSPACES**

-  Wheeled Play - Skate Park
-  Play Area

**BUILDING HEIGHTS**  
(Ref: ES Parameter Plan: SCALE)

-  Up to 2 Storeys (9m)
-  Up to 2.5 Storeys (11m)
-  Up to 3 Storeys (12m)



## GREEN INFRASTRUCTURE

### **Connectivity and access to the countryside**

7.4.6. The Landscape Institute defines GI as:

*“...the network of natural and semi natural features, green spaces, rivers and lakes that intersperse and connect villages, towns and cities. Individually these elements are GI assets, and the roles that these assets play are GI functions. When appropriately planned, designed and managed, the assets and functions have the potential to deliver a wide range of benefits – from providing sustainable transport links to mitigating and adapting the effects of climate change.”*

7.4.7. The Landscape Masterplan captures the key aspirations that have been worked up for the site by the working group that have prepared the ‘Worcestershire Sub-Regional Green Infrastructure Framework: Foxlydiate Site Concept Plan’. Extensive collaborative working with the GI group’s key stakeholders: the County and District Councils, has informed the new landscape of the development.

7.4.8. The Landscape Strategy delivers the GI linkage set out in the Concept Masterplan. The points of connection with Cur Lane and Gypsy Lane will be made in a manner that is sensitive to the qualities of the landscape at the countryside edge and will sit in attractive open space.

### **The network of new green spaces**

7.4.9. The GI strategy within the Landscape Masterplan looks to take best advantage of the diversity of habitat already present already in the landscape of the site including: the hedgerows and field pattern; the copses and Hawthorn Pit; and the Spring Brook corridor.

7.4.10. These components will be taken into the new landscape with an approach that looks to realise the potential of both individual components: the tree cover of the Hawthorn Pit copse for example, as well as the real network gains through a strategy of connection that makes links through the new landscape.

### **Boundaries and the fit in the landscape**

7.4.11. The historic lines of the lanes at the site edges: Cur, Gypsy and Pumphouse Lanes, will form the limits to the growth of the development to the south, west and northwest. Development is to sit back from these edges behind new naturalistic green spaces.

7.4.12. The earthworks of the A448 form the north eastern edge. All of these routes have substantive vegetation: hedges and hedgerow trees and in the case of the A448 extensive tree cover. These historic routes and boundaries will act as a framework for the development.

7.4.13. The Landscape Masterplan will make a sympathetic and successful fit. It is not contriving a line that does not already exist. The scheme successfully fits into the landscape and will cause a strengthening of the existing elements at the edges, defining a strong and intelligent edge of the scheme.

### **Redditch New Town: growth in a landscape setting**

7.4.14. The County GI Framework study looks at the designation of Redditch as a ‘New Town’ stating that:

*“The visual and physical containment of new residential, industrial and infrastructural development was seen as a priority in order to conserve the distinctive rural environments of the surrounding landscape.” And “The New Town was therefore highly successful at ensuring landscape and environmental character were both conserved and enhanced, preserving a strong distinctiveness between urban and rural landscapes, and retaining a key amenity for the residents of Redditch, despite the large-scale urban expansion.”*

7.4.15. The GI strategy will continue to apply this aspiration for distinctiveness and containment.





## 7.5. THE PLAY PROVISION STRATEGY

*National guidance on play space and sports fields for new developments has moved the strategies for provision away from an over simplistic calculation-based rationale to the ‘Fields in Trust’ approach that looks to a greater and closer examination of location and population need: both existing and projected.*

7.4.16. The Play Provision Strategy for the development is contained within the Concept Masterplan and the Landscape Masterplan demonstrates how the play spaces will sit within the open spaces of the scheme. Key considerations are the quality of the facility and the safety of the connections between the play spaces: the parks, the central play area and the trails. All of the play spaces are connected through off-road green corridors. The connections are to be made within a network that utilizes the historic hedgerow boundaries in combination with new network features that are part of the public open space framework of the new scheme.

7.4.17. Play space and the provision of outdoor activity areas have been the subject of numerous workshops and discussions with Council Officers. The strategy for outdoor play space for the development has emerged through this consultative and collaborative process.

7.4.18. The proposal is for a range of play and exercise facilities: a wheeled play area; equipped play spaces that will provide for age ranges and types of play from modest and low key with a ‘doorstep-play’ quality; to the centralised highly social large scale

‘play park’. The provision of formal sports pitches will be accommodated away from the site but the development will have a circular exercise trail that makes real advantage of the network and the variety in the character of the landscape.

7.4.19. The central ‘play park’ has been promoted by Officers at BDC and it is proposed in a location that has a clear logic: it is central and well connected. It is to sit between one of the new greenway open spaces and will be linked via an easy, car-free and attractive walk through the large new public park: ‘Cur Lane Park’, to the proposed local centre and school.

7.4.20. This concentration of play experience enables the Council to have a proven successful role in management and maintenance. This strategy also limits potential friction between future near residents and users of the play space. A large, exciting and popular play facility is an entirely expected component of a park and the intention is for the ‘play park’ to be regarded as a real asset to the development.





-  Proposed main road within FOS
-  Proposed main road route to be located within areas of residential development and the existing linkway
-  Proposed bus stops
-  NEAP
-  LEAP
-  LAP
-  Potential area for informal football / outdoor play



CUR LANE PARK







Cur Lane Park: Concept



NORTHERN PARKLAND



*Northern Parkland: Concept*





SPRING BROOK PARK



Spring Brook Park: Concept

## 7.6. DRAINAGE

*The surface water strategy will aim to mimic the existing drainage regime as far as possible, to ensure flood risk is not increased. This will be achieved through the use of Sustainable Drainage Systems (SuDS).*

7.6.1. Surface water runoff from the site will discharge into the Spring Brook, restricted to greenfield runoff rates, including an allowance for climate change. To achieve this, a number of attenuation facilities will be provided, in the form of open ponds where possible. Source control SuDS will also be implemented where appropriate, such as permeable paving, or water butts, which through filtration and settlement, will contribute to water quality improvements. The surface water drainage system will

provide ecological, amenity and aesthetic benefits, as well as ensuring flood risk is not increased elsewhere.

7.6.2. All new sewerage infrastructure will be built in accordance with the latest version of Sewers for Adoption.

7.6.3. Full details of the proposed drainage strategy are contained within the FRA which forms part of the planning application package.



Drainage Strategy drawings





**REFERENCE**

- SITE BOUNDARY
  - ▶ INDICATIVE SURFACE WATER SEWER ROUTE
  - EXISTING DITCH
  - DITCH DIVERSION
  - ▶ GENERAL FALL (BASED ON EXISTING SITE TOPOGRAPHY)
  - N CATCHMENT DISCHARGING TO RELEVANT ATTENUATION AREA
  - STW BOREHOLE ABSTRACTION ZONE (NO INFILTRATION ALLOWED)
  - - - 2650 PIPELINE
  - - - GAS MAIN
  - - - WATER MAIN
  - - - STANDOFF FROM SERVICES
  - S → PHOTO VIEW LOCATION
- FLOOD EXTENTS:**
- 1 IN 20YR RETURN PERIOD
  - 1 IN 100YR RETURN PERIOD
  - 1 IN 100YR RETURN PERIOD + 20%
  - 1 IN 1000YR RETURN PERIOD



## 7.7. PHASING

*A proposed Phasing Strategy has been prepared and which demonstrates that the delivery of development would proceed from multiple access points. This will enable different sales outlets from housebuilders to proceed across different parts of the site concurrently to ensure that the development is completed by 2030.*

7.7.1. Initially, development would proceed from Phase 1, creating the main site access from A448 with additional phases coming on stream relatively quickly. The key community facilities including the local centre, first school and central areas of open space would all fall within Phase 1.

7.7.2. The level of housing within each phase is set out in the table below;

7.7.3. On this basis, a delivery trajectory has been developed and which would support the delivery of private completions across the plan period. Delivery would peak at around 150 private sales per year which would be achieved from 4 sales outlets operating

simultaneously with around 3 to 4 sales per month each. Affordable housing would be delivered concurrently on a phase by phase basis in partnership with affordable housing providers.

7.7.4. The build trajectory is set out below:

7.7.5. Whilst delivery rates would be initially relatively modest, this would increase rapidly as more phases of development become available.

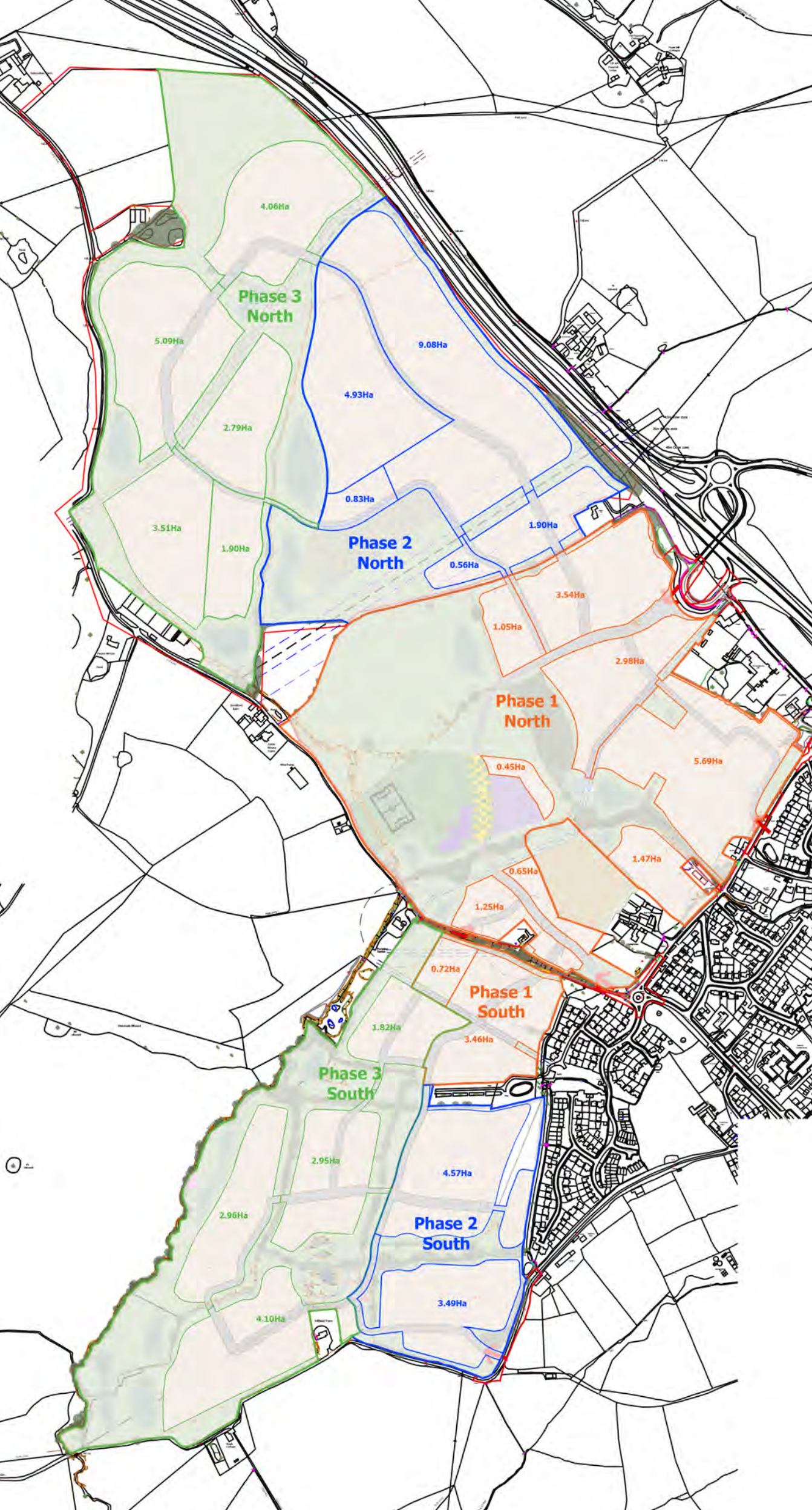
7.7.6. Further details in respect of phasing and the delivery of key infrastructure and s106 planning gain is set out in the Planning Statement by GVA.

### *Land at Foxlydiate Lane, Sales Trajectory during the period 2016-2030*

Date (April- March)	Year	Private Completions	Affordable Completions
2016 - 2017	1	0	0
2017 - 2018	2	50	33
2018 - 2019	3	100	69
2019 - 2020	4	130	79
2020 - 2021	5	140	89
2021 - 2022	6	150	102
2022 - 2023	7	150	102
2023 - 2024	8	150	102
2024 - 2025	9	150	102
2025 - 2026	10	150	102
2026 - 2027	11	150	102
2027 - 2028	12	120	79
2028 - 2029	13	120	79
2029 - 2030	14	120	79
2015 - 2030		1680	1120

Source: Billfinger GVA





	Site Boundary (136.03 ha)
	Phase 1
	Phase 2
	Phase 3

**Phase 1 North - 38.50Ha**  
(Development land - 17.08Ha)

3.54Ha	49Dph	173
1.05Ha	32Dph	34
2.98Ha	49Dph	146
0.45Ha	45Dph	20
5.69Ha	38Dph	216
1.47Ha	37Dph	54
0.65Ha	45Dph	29
1.25Ha	45Dph	56

Total units	728
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**Phase 1 South - 4.43Ha**  
(Development land - 4.18Ha)

0.72Ha	35Dph	25
3.46Ha	45Dph	156

Total units	181
-------------	-----

Sum total PH1	909 units
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**Phase 2 North - 24.54Ha**  
(Development land - 17.30Ha)

9.08Ha	40Dph	364
4.93Ha	42Dph	207
0.83Ha	32Dph	27
0.56Ha	30Dph	17
1.9Ha	35Dph	67

Total units	682
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**Phase 2 South - 11.01Ha**  
(Development land - 8.06Ha)

4.57Ha	32Dph	146
3.49Ha	32Dph	112

Total units	258
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Sum total PH2	940 units
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**Phase 3 North - 31.14Ha**  
(Development land - 17.35Ha)

4.06Ha	28Dph	114
5.09Ha	30Dph	153
2.79Ha	44Dph	123
3.51Ha	30Dph	105
1.90Ha	35Dph	67

Total units	562
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**Phase 3 South - 24.68Ha**  
(Development land - 11.83Ha)

1.82Ha	37Dph	67
2.95Ha	35Dph	103
2.96Ha	30Dph	89
4.10Ha	27Dph	111

Total units	370
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Sum total PH3	932 units
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<b>Total on site units inc. 19 in mixed use area</b>	<b>2800</b>
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Indicative Phasing plan







*Chapter 8:*

# ILLUSTRATING THE QUALITY OF DESIGN: DEVELOPMENT FORM

## 8.1. APPROACH TO CHARACTER

*The proposals for Monarch Green will comprise a distinctive character and a strong sense of place, guided by important site features and an analysis of existing development within Webheath, Redditch and surrounding settlements.*

### LEGIBILITY FRAMEWORK

8.1.1. The Legibility framework plan establishes a number of important character generators for the development. These character generators support the urban design principles that underpin the concept plan. They will further shape the approach to character and the creation of a distinctive development.

8.1.2. This plan provides advice at a strategic level of importance. It is not exhaustive, and at a detailed level smaller areas are likely to be identified that require additional design consideration.

8.1.3. Key character generators are set out below and illustrated on the Legibility Framework Plan;

#### **Landmark Building**

8.1.4. A landmark building will truly define the character of the development at Monarch Green. They aim to provide a distinctive building that marks the heart of the proposals. The use of a landmark building within the local centre will aid legibility and further emphasise the creation of a new community hub.

#### **Key Buildings**

8.1.5. Key buildings help to aid legibility, define spaces and streets and provide attractive points of interest along the street scene. They could be defined by an increased storey height or variation in architectural material or detail. Key buildings located within the development are generally located on a corner and used to terminate an important view along the primary movement route or to open space.

#### **Focal and Entrance Spaces**

8.1.6. Focal and entrance spaces will be generally defined by important frontages, the use of key/ landmark buildings and considered tree planting and public realm design.

8.1.7. The local centre plaza forms a key focal space for the development that marks the heart of the new community at Monarch Green. The plaza will form an attractive active hub to pass through or stay and enjoy.

#### **Green Node**











8.1.8. Green nodes are areas which encourage activity and interaction, ensuring the creation of a vibrant new community. They will be accessible to the weight of population within the northern, central and southern areas of the development and benefit from good levels of enclosure and natural surveillance.

#### **Important Frontage**

8.1.9. Important frontages are identified in locations along primary and secondary movement routes where the building frontage is integral to creating a sense of arrival, enclosure, and forming part of a long distance view along the street. They will also help to create a series of 'street pictures' through consideration of street form, width, setback and roofline.





-  Landmark Buildings
-  Key Buildings
-  Facial axes
-  Entrance axes
-  Green nodes
-  Uncertain frontage
-  Green edge
-  Parkland edge
-  Views along important building frontage
-  Open views

Legibility Framework plan



### ***Green and Parkland Edge***

8.1.10. Green and parkland edges aim to provide an attractive and sensitive interface between key open spaces and the built form. These edges could be defined by planting, larger front gardens and a considered palette of materials and architectural details, ensuring the creation of an attractive and appropriate frontage.

8.1.11. Green edges are identified along the south western edge of the site, where development adjoins Spring Brook. The use of green edges in this location will help to aid a sensitive transition between open space adjacent to the Brook and built development.

8.1.12. Parkland edges will be located in central and northern areas of the site. They provide an important transition between development and parkland that is located within the site to ensure that the setting of buildings of historic note are respected.

### ***Views along Important Building Frontage***

8.1.13. Establishing these views helps to identify other character generators, in particular, key/landmark buildings and important building frontages. The proposals aim to retain and enhance these views.

### ***Open Views***

8.1.14. In addition to views along important building frontages, a number of open views within the site have also been identified. They tend to be located on areas of higher ground; their retention is valuable to ensure visual connectivity across the site and aid site specific character.

## **CHARACTER AREAS**

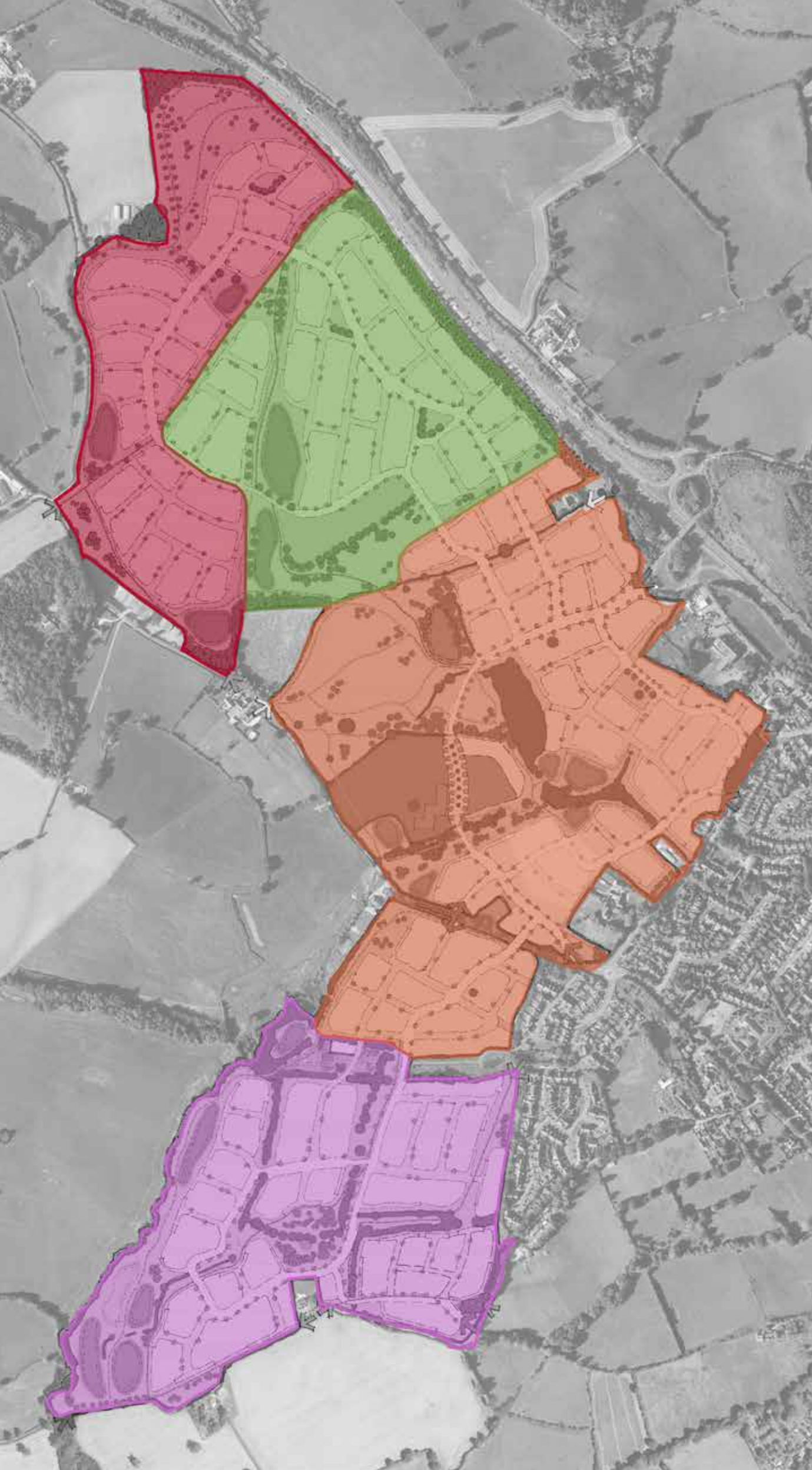
8.1.15. The development proposes six character areas which seek to respond to existing site features, location and variations in density, scale and formality. The character areas location plan shows the boundaries of the four proposed character areas. They are;

- » Holyoake Park
- » Cur Green
- » Monarch Green Village
- » Spring Rise

8.1.16. A number of key spaces that strongly contribute towards the character and distinctiveness of the development will also be identified within each character area.

8.1.17. Identification of positive character elements in Webheath, Redditch and surrounding settlements set out in section 4.8 will also inform the character of the proposed development.





- Holyoake Park
- Car Green
- Monarch Green Village
- Spring Rise

*Character Areas Location plan*



## DENSITY STRATEGY

*8.1.18.* A variety of densities are proposed across the development; this will also help to aid the creation of distinct character areas. These densities accord with site context, building heights and proposed land uses. A series of density ranges are proposed to ensure that a significant variation in density can occur within development parcels, as appropriate.

*8.1.19.* Overall, development will predominantly range from 25 - 40 dph (dwellings per hectare).

*8.1.20.* Lower densities (25 – 30dph) are located in the most northern and southern areas of the site where the aspiration is to create a more informal and rural edge character. Density levels in these areas also respond to important site features, including topography and existing green infrastructure. However, these proposed density levels will still ensure good levels of enclosure and natural surveillance are created along movement

routes and significant areas of open space.

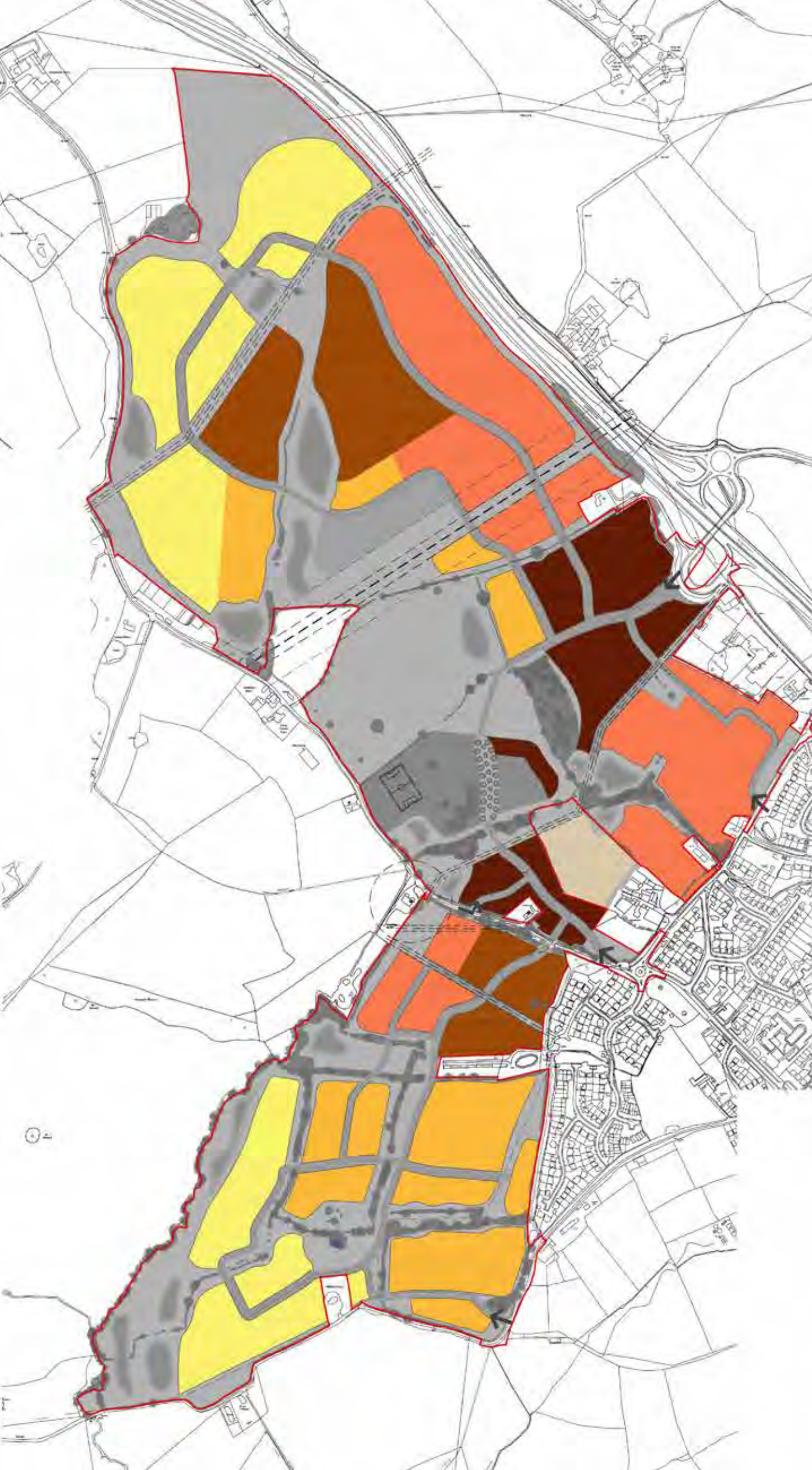
*8.1.21.* Medium densities (30 – 40dph) are located in the following areas;

- » In the northern area of the site to aid the creation of a suburban character and parkland frontage.
- » Central areas of the site to reflect existing development located in Webheath.
- » Southern areas of the site where the structure of development is permeated by a network of green corridors.

*8.1.22.* A variation between 30 – 40dph ensures that densities can vary in accordance with open space, site edges, adjoining existing development and the primary movement route.

*8.1.23.* Areas of higher density development (40 – 50dph) are predominantly located within the central and northern areas of the site. This responds to the location of local centre, primary movement route (including key entrance points to the development) and focal areas of public open space. Areas of higher density development aim to aid the creation of a formal character and aid legibility.





Density:

-  25 to 30 dph
-  30 to 35 dph
-  35 to 40 dph
-  40 to 45 dph
-  45 to 50 dph
-  45 to 50 dph



## 8.2. HOLYOAKE PARK

*Holyoake Park will be defined by a generosity of space, the use of low densities and the northern parkland. It will reflect a traditional style that takes precedent from the local farmhouse vernacular.*

- » Provision of large development blocks that are structured in accordance with the topography. The use of long and straight streets ensure views to green space within the development and to the wider countryside.
- » Parkland edges are a defining feature of the character area. They could be characterised by the use of large front gardens and supplemented planting, with storey heights restricted to 2 storeys.
- » The northern parkland area is a key space; this will aid the creation of a green and rural character. It is linked to green corridors that adjoin the edge of the site.
- » Low density development (25 – 30dph) to aid a generosity of green space.
- » 2 storeys dwellings will be located in the majority of the character area; aiding a rural character and respecting views from the site to the wider area.
- » Predominantly detached dwellings with some semi-detached properties. Dwellings will be located in large plots.
- » General use of simple and traditional building forms.
- » The style of character area takes cues from the local farmhouse vernacular and materials.
- » Predominant use of red brick facades.
- » Front gardens are generally larger; residential boundary treatments will be defined with hedgerows.







*Holyoake Park illustrative visual*

## 8.3. CUR GREEN

*Cur Green will have a formal character that is defined by topography, a contemporary style and a network of long and straight streets that provide long distance views to the central parkland.*

- » Development is organised around a grid structure of long and straight streets, aiding the creation of a formal character and long distance east – west views to green space.
- » The northern avenue passes through the character area, its alignment is curving in contrast to the tertiary network of long and straight streets. This alignment aids the creation of terminating views to key buildings and allows for small scale spaces to be located along this route.
- » Significant areas of open space (the central parkland) are located on the western edge of the character area; a linear green corridor is integrated with development.
- » Higher densities (40- 45dph) are located centrally within the character area; density decreases in accordance with the topography and central parkland frontage to the east and west respectively.
- » General use of increased storey heights (2.5 – 3 storeys) with 2 storey dwellings fronting on to the central parkland.
- » Predominant use of semi-detached and terraced dwellings located within smaller plots.
- » Less variation in architectural details to aid a sense of rhythm along the street-scene and a formal character.
- » A more contemporary style with a mix of brick and render facades with red tiles.
- » Front gardens are generally smaller; residential boundary treatments will be defined with hedgerows.







Cur Green illustrative visual

## 8.4. MONARCH GREEN VILLAGE

*Monarch Green Village will form the heart of the development and is the most active area of the site; providing access to the local centre, first school and central parkland. It will be characterised by the use of some contemporary building forms, increased storey heights and areas of development linked by an attractive boulevard.*

- » Monarch Green Village forms the high density core of the development, ensuring that the weight of population is close to green infrastructure and local facilities.
- » Areas of high density development (40 – 50dph) are contrasted by significant areas of open space that include central informal parkland and the Hawthorn Pit, aiding the creation of a distinctive character.
- » The character area generally comprises a formal and ordered arrangement of development blocks and tertiary streets that are structured around the route of the boulevard.
- » The use of long and straight streets aids a more formal character.
- » Predominant use of terraces and apartment located within smaller plots.
- » Use of increased storey heights (2.5/3 storeys) within the local centre and along the boulevard and avenue to aid a sense of enclosure and to mark key/landmark buildings. 2 storey buildings will be located where it is appropriate to reflect existing built development in Webheath.
- » Use of more contemporary building forms located within the local centre and along the boulevard and avenue. More traditional and suburban building forms could be located where the site adjoins existing development in Webheath.
- » Potential for more formal residential boundary treatments such as railings along the boulevard.



Monarch Green Local Centre - Illustrative Sketch Layout





- |  |                              |   |                                     |   |  |
|--|------------------------------|---|-------------------------------------|---|--|
|  | Primary School               |  | Health Centre<br>(Apartments above) |  | Storey Heights                           |
|  | Community Hall               |  | Dwellings                           |  | Existing trees/hedges                    |
|  | Retail<br>(Apartments above) |  | Apartments                          |  | Proposed Trees/hedges<br>(Retain/Locate) |









*Illustrative aerial view of the proposed development at Monarch Green*



## 8.5. SPRING RISE

*Spring Rise will reflect a low density and informal character. It will provide an attractive green edge between the development and the Spring Brook green corridor.*

- » Varied and organic block structure that is shaped by existing green infrastructure.
- » Proposed densities (30 – 35dph) reflect the existing built form in Webheath. Lower densities (25 – 30dph) will be located in the western edge of the character area, adjoining Spring Brook.
- » Storey heights accord with the density strategy; 2 storey dwellings will be located adjacent to the Spring Brook and Pumphouse Lane. Increased storey heights (up to 2.5 storeys) could be located within the eastern side of the character area where appropriate.
- » The use of curving streets and potential variations in the building line and setback aids a more informal character.
- » A mix of detached and semi-detached dwellings located within larger plots.
- » Establishment of a continuous frontage along primary and secondary movement routes that could include the use of wider fronted properties.
- » Enhancement and supplementation of the existing green infrastructure to fully integrate development within the landscape.
- » Traditional building forms with render, red brick and slate roofs.







Spring Rise illustrative visual







*Chapter 9:*  
SUMMARY



## 9.1. SUMMARY

*This Design and Access Statement has set out a clear explanation of the design process and comprehensive consultation with the Local Planning Authorities, stakeholders and the community. A summary of how the main vision objectives will be met is set out here;*





## GENEROUS GREEN INFRASTRUCTURE

*9.1.1.* A landscape led approach has enabled the creation of a masterplan that is shaped by the topography, views and existing blue and green capital located on the site.

The scheme will comprise a significant amount of open space (approximately 53ha) that will cater to a range of uses and ages, promoting healthy, happy living through opportunities for physical exercise, recreation and local food growth.

*9.1.2.* A number of key spaces have been established within the site, as presented on the Green Infrastructure plan. These spaces will ensure the creation of an attractive and distinctive character and will be connected via an accessible network of green corridors that incorporate pedestrian and cycle routes.

*9.1.3.* This approach aims to bring benefit to the new and existing community and also ensure that the natural, built and historic environment, including ecology and biodiversity, is respected and enhanced wherever possible.



## CONNECTED

*9.1.4.* The masterplan has been designed to ensure the provision of accessible, legible and safe links across the site and to wider destinations for all modes of transport. Moving around the site by bus, bike or on foot will be an attractive option, with shared off road cycle and footpaths being integrated within green corridors and the design of primary and secondary movement routes. Consideration has also been given with regard to how sustainable linkages to Webheath and Redditch can be provided as part of the proposals.

*9.1.5.* The local centre and first school are co-located in the most central part of the site; ensuring that this becomes a focal space for the Monarch Green development and is easily accessible for new residents and the existing community. Strategies and principles for ensuring that this space functions as a vibrant and active community hub are set out in the latter chapters of the DAS.



## DISTINCTIVE

*9.1.6.* Monarch Green will form an attractive and distinctive landmark development for Webheath and Redditch. The DAS has set out an analysis of the site and surrounding area; identifying important site features and positive elements of the existing built form that contribute towards a distinctive local character. This has enabled the creation of a site specific development with a strong sense of place.

*9.1.7.* The proposed character areas set out in section 8 present a series of illustrative visuals to show how a unique sense of place could be achieved. This has been guided by the existing character area analysis set out in section 4 and the character generators presented on the legibility framework plan.



## A NEW GARDEN NEIGHBOURHOOD

*9.1.8.* A cohesive new community will be created at Monarch Green, encouraged through the provision of an accessible local centre and recreation facilities available on the doorstep. The masterplan is based on best practice urban design and garden city principles, considering elements including land uses, accessibility, permeability, legibility, character, street design and landscape.

*9.1.9.* The masterplan is underpinned by comprehensive technical surveys and input, as set out in sections 3 and 5 of the DAS, with detailed reports included in the planning application package. This will ensure the delivery of residential dwellings in accordance with the housing supply requirement identified by the Local Planning Authorities. The strategy for phasing, along with proposed densities is set out in section 7.7 of the DAS.



9.1.10. The parameters and supporting strategy plans demonstrate how the vision set out at the start of the DAS can be delivered. The DAS has also included;

- » The establishment of a vision and vision objectives.
- » Presentation of the masterplan cascade and parameter plans for which outline planning permission will be sought. These plans should be read in conjunction with additional plans that are submitted as part of the detailed application package.
- » A comprehensive assessment of the site and its context to identify important site features and existing design elements which are influential in shaping the character of Webheath, Redditch and surrounding settlements.

» Production of a concept plan which seeks to deliver the vision, respond to assessment work and feedback from the local community, Local Planning Authorities and stakeholders.

» Development of clear design strategies which establish a framework for detailed design proposals going forward.

9.1.11. In conjunction with the parameter plans a number of strategy plans may be identified as forming part of the outline planning application for the site by the Local Planning Authorities in agreement with the applicants. These strategy plans are presented opposite and include;

- » Density
- » Street Hierarchy
- » Foot, Bus and Bike
- » Legibility Framework
- » Play Strategy
- » Residential Phasing



## STATUS

Fixed for  
Hybrid Planning  
Application



## STATUS

Strategy plans that  
may be identified  
for approval (in  
agreement with the  
applicants) by the  
LPA. They include:

- » Density
- » Street Hierarchy
- » Foot, Bus and Bike
- » Legibility Framework
- » Play Strategy
- » Residential Phasing



